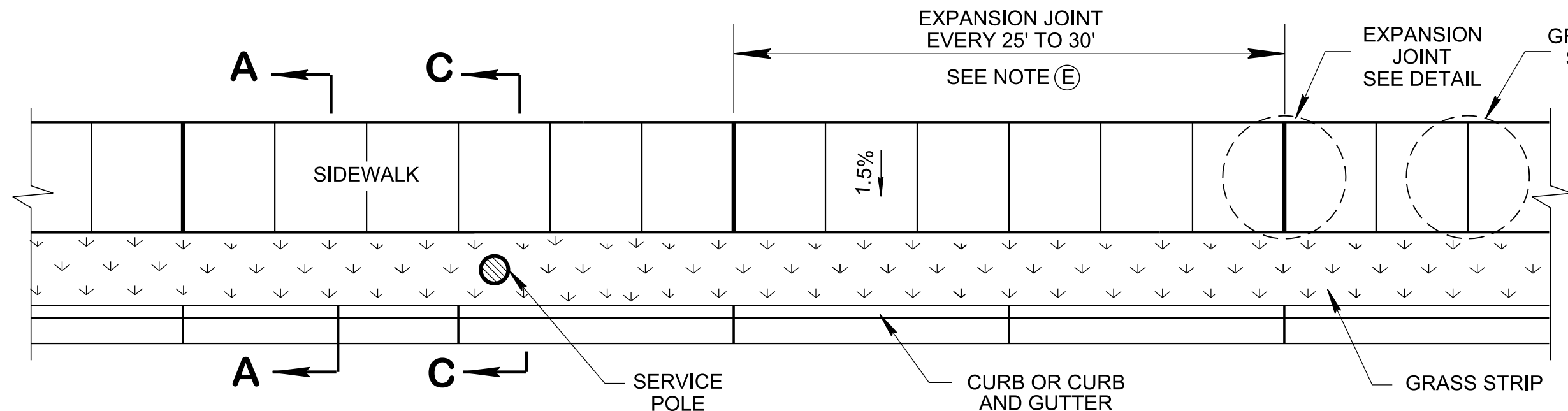
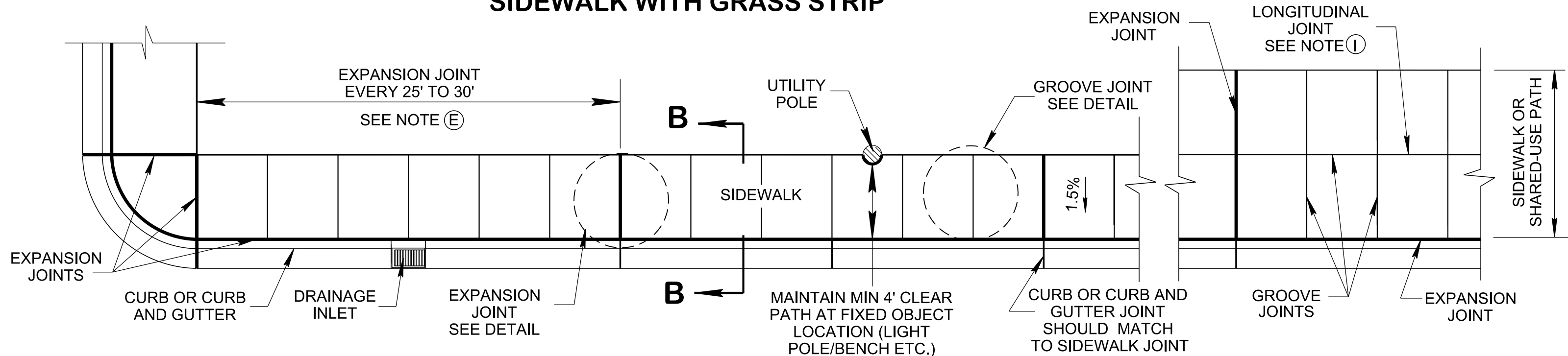


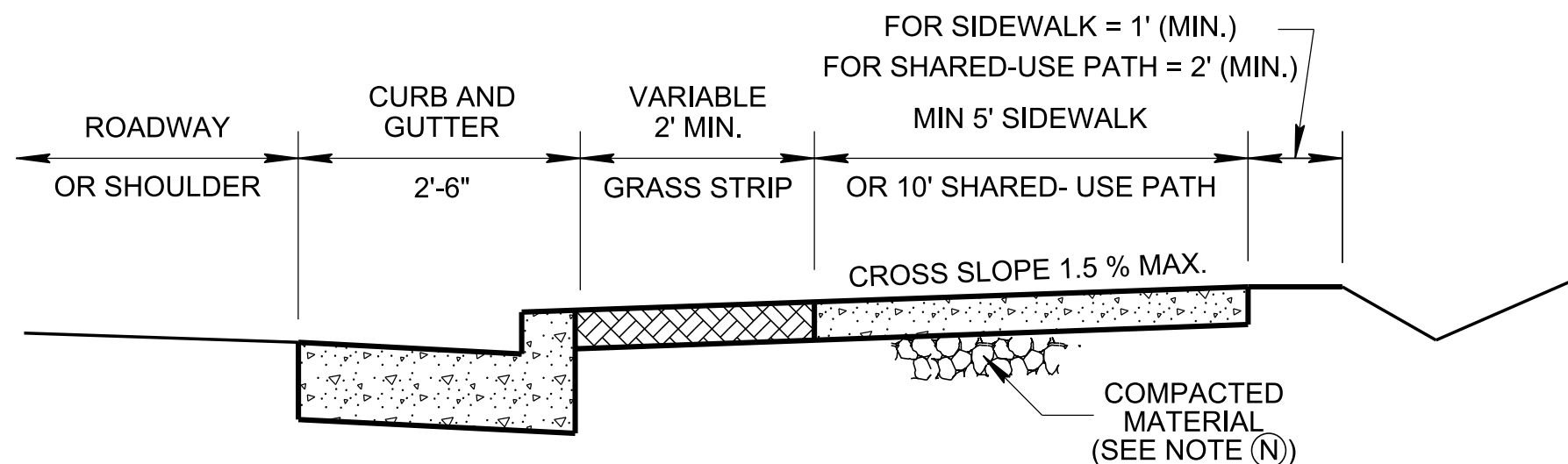
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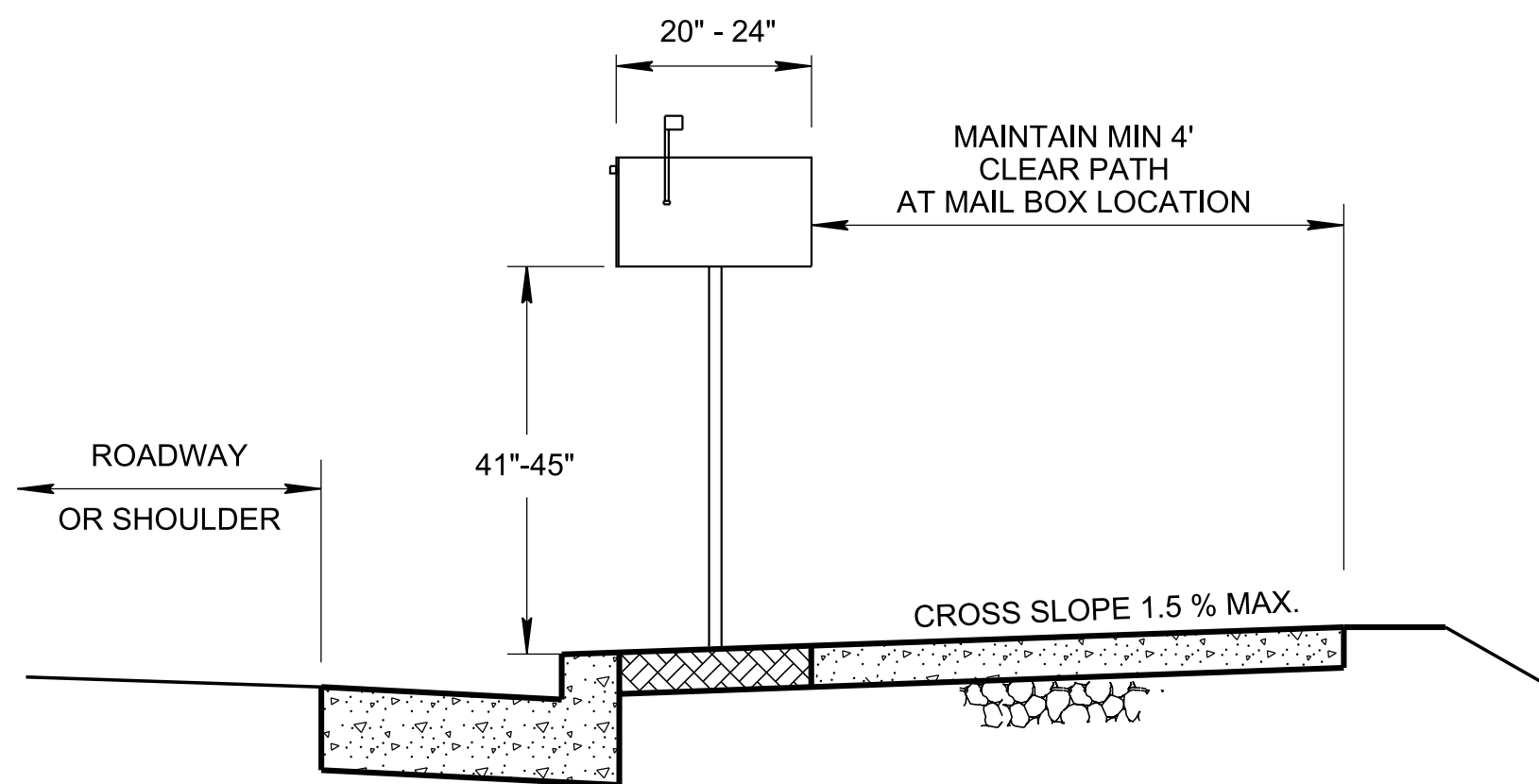
PLAN VIEW
SIDEWALK WITH GRASS STRIP



PLAN VIEW
SIDEWALK WITHOUT GRASS STRIP
(NOTE: WHEN NEEDED DUE TO EXTREME SITE CONDITIONS THE PLACEMENT OF SERVICE APPURTENANCES SHALL PROVIDE MINIMUM 4' CLEAR PATH.)



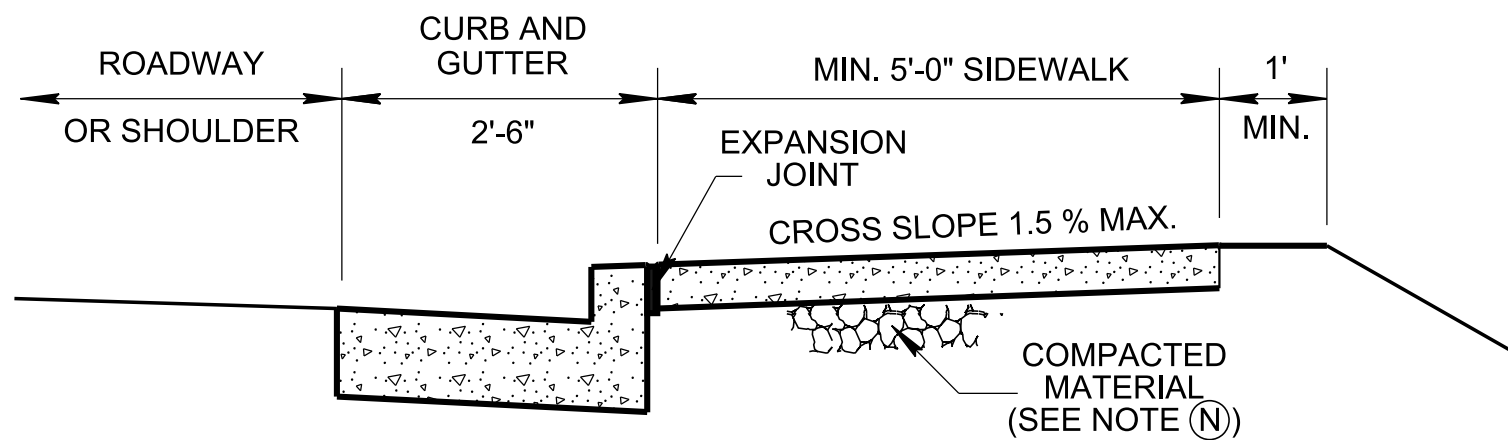
SECTION A-A
TYPICAL SIDEWALK CROSS SECTION
WITH GRASS STRIP



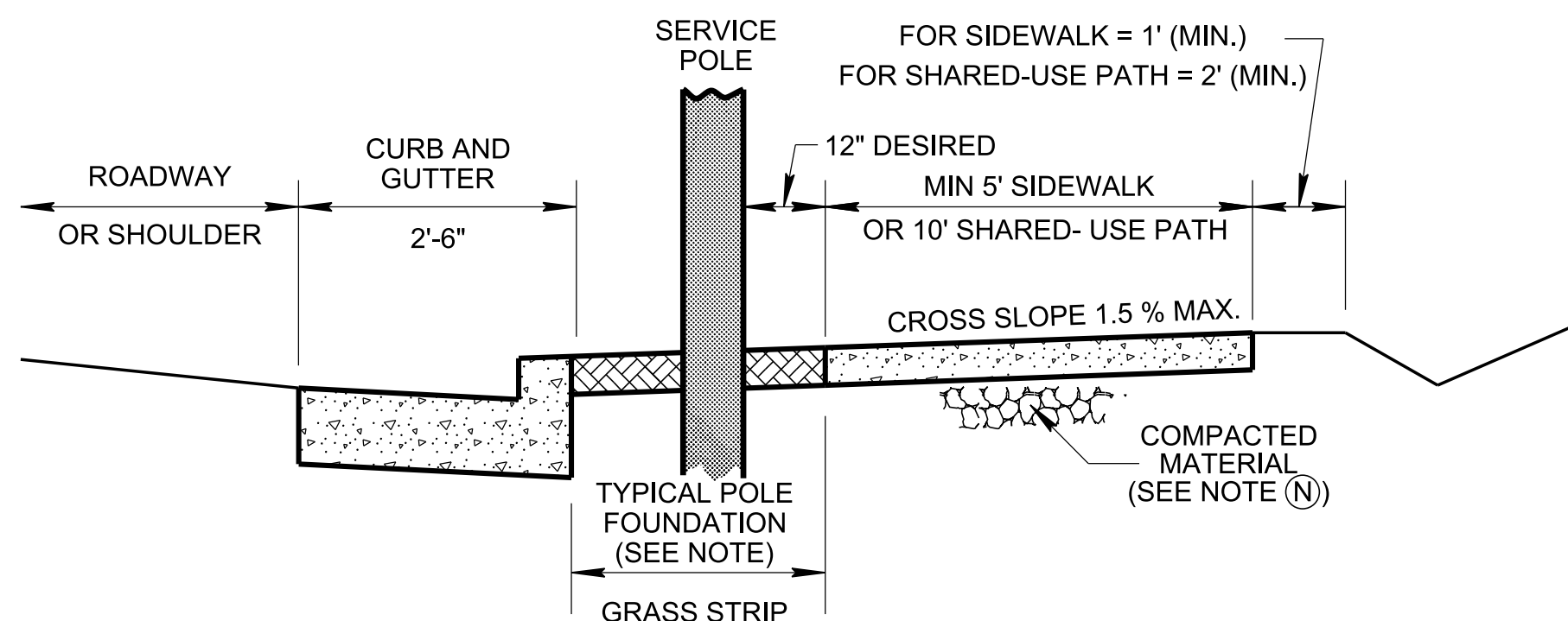
MAILBOX DETAIL

NOTE: EDGE OF MAILBOX SHALL NOT OVERHANG BEYOND THE BACK OF THE CURB.
NOR SHALL THE MAILBOX OVERHANG THE SIDEWALK SUCH THAT THE USABLE WIDTH IS LESS THAN 4 FEET.

IF NEEDED, REMOVAL AND RESETTING MAILBOXES TO BE INCLUDED IN THE COST OF SIDEWALK.



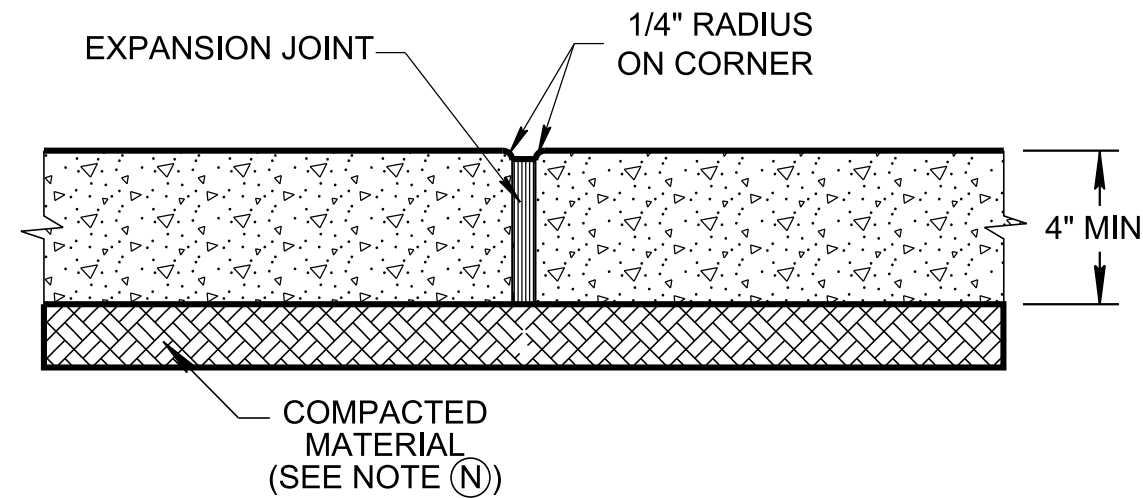
SECTION B-B
TYPICAL SIDEWALK CROSS SECTION
WITHOUT GRASS STRIP



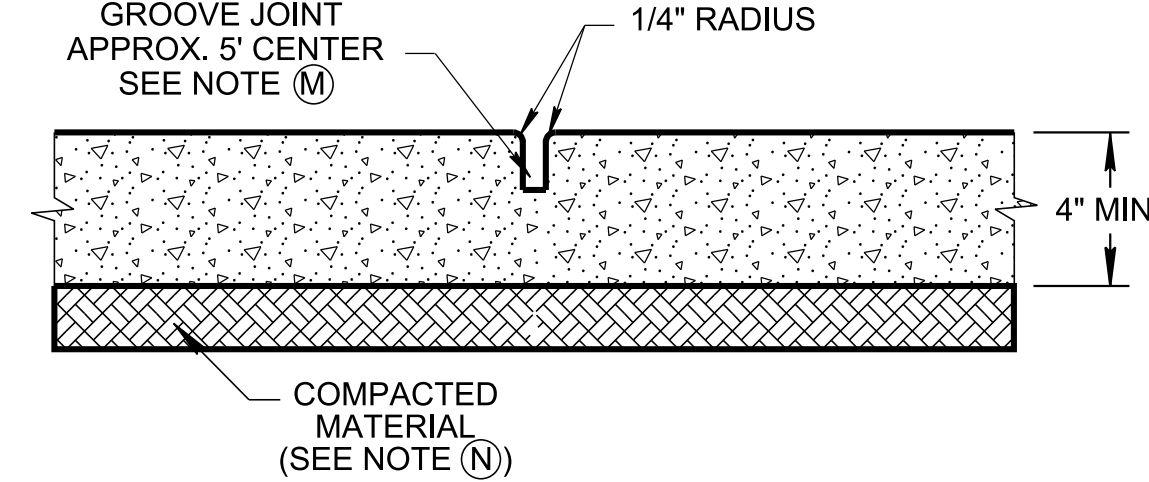
NOTE: IF SERVICE POLE IS PLACED IN GRASS STRIP THE POLE SHALL HAVE MIN. 3' OFFSET FROM TRAVELLED LANE. MINIMUM 36" DIAMETER AND 15' DEEP FOUNDATION SPACE SHALL BE EVALUATED TO ELIMINATE CONFLICTS. SEE STANDARD TRAFFIC OPERATION DRAWINGS T-SG-9 AND T-SG-10 FOR MORE INFORMATION.

SERVICE APPURTENANCES (LARGE SIGNS, STRUCTURES, SIGNAL, LUMINARY AND UTILITY POLES 2' DIAMETER OR LARGER) SHALL BE PLACED OUTSIDE THE PEDESTRIAN ACCESSIBLE SPACE, PREFERABLY OUTSIDE THE SIDEWALK AREA AND INSIDE THE RIGHT-OF-WAY.

SECTION C-C
TYPICAL SIDEWALK CROSS SECTION
WITH GRASS STRIP AND SERVICE APPURTENANCES



EXPANSION JOINT DETAIL



HAND TOOL GROOVE JOINT DETAIL

REFERENCED STANDARD DRAWINGS

- SEE T-M-4, FOR CROSS WALK MARKING
- SEE MM-CR SERIES FOR CURB RAMP DETAILS
- SEE MM-BPR-1, FOR PEDESTRIAN RAIL REQUIREMENTS & S-PL-6, FOR GUARDRAIL PLACEMENT
- SEE MM-SW-2, FOR ALTERNATE DETAILS FOR CONCRETE SIDEWALK (REHABILITATION)
- SEE RP-SC-1, FOR 6" SLOPING CONCRETE CURBS AND CONCRETE CURBS AND GUTTERS
- SEE RP-VC-10 OR 11, FOR VERTICAL CONCRETE CURB AND CONCRETE CURBS AND GUTTER DETAILS
- SEE MM-PM-1 THRU MM-PM-5, FOR BIKE LANE/ROUTE PAVEMENT MARKINGS
- SEE RP-D-15 & 16 FOR CONCRETE DRIVEWAYS
- SEE MM-TS-2 FOR PEDESTRIAN FACILITY LATERAL OFFSETS/ BUFFER GUIDANCE.
- SEE MM-TS-3 FOR SHARED USE TYPICAL SECTIONS

GENERAL NOTES

- (A) ALWAYS PLACE SIDEWALK AS FAR AS AWAY FROM THE TRAVELLED WAY WHEN POSSIBLE. FOR SPECIFICATIONS SEE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION.
- (B) WHERE IT BECOMES NECESSARY TO REMOVE PARTS OF EXISTING CONCRETE SIDEWALKS OR RAMPS, THE RESULTING EDGES SHALL BE CUT TO A NEAT LINE, AND ANY OFFSETS IN SUCH LINES SHALL BE MADE AT RIGHT ANGLES.
- (C) SIDEWALK WIDTHS DO NOT INCLUDE THE SIX INCH CURB WIDTH OF PROPOSED TOP OF CURB.
- (D) MAXIMUM SIDEWALK CROSS SLOPE IS 1.5 %. ALL SIDEWALKS SHALL HAVE A BROOM FINISH AND SHALL BE 4" THICK UNLESS THE PLANS CALL FOR 6" THICKNESS. THE CONCRETE SHALL BE CLASS "A" AT 3000 PSI. ALL COST TO BE INCLUDED IN ITEM NO. 701-01.01, CONCRETE SIDEWALK (4"), S.F. OR 701-01.02, CONCRETE SIDEWALK (6"), S.F.
- (E) EXPANSION JOINTS ARE TO BE PLACED 25 TO 30 FEET APART DEPENDING ON TRANSVERSE JOINT MARKINGS AND NEED TO MATCH CURB EXPANSION JOINT WHERE SIDEWALK IS BUILT DIRECTLY AGAINST CURB, OR AS DIRECTED BY THE ENGINEER WHERE THE PROPOSED SIDEWALK IS IN CONTACT WITH THE STREET RETURNS, ON BUILDING LINES PRODUCED AT STREET INTERSECTIONS, WHERE WALKS LEAD TO HOUSE OR OTHER ENTRANCES AND AN OTHER LOCATIONS WHERE STRESSES MAY DEVELOP. THE COST OF ALL EXPANSION JOINTS IS TO BE INCLUDED IN THE UNIT PRICE BID FOR THE PROPOSED SIDEWALK.
- (F) CONCRETE JOINT MATERIAL TO BE FLUSH WITH THE SIDEWALK SURFACE, ONE INCH PREFORMED FILLER IN ACCORDANCE WITH SECTION 701.06 OF THE STANDARD SPECIFICATIONS.
- (G) ONE INCH EXPANSION JOINTS ARE TO BE PLACED WHERE THE PROPOSED SIDEWALK IS IN CONTACT WITH CIRCULAR CURBS, BUILDINGS AND/OR RETAINING WALLS.
- (H) HALF INCH EXPANSION JOINTS ARE TO BE USED AT ALL OTHER LOCATIONS.
- (I) LONGITUDINAL JOINT MARKINGS WILL NOT BE REQUIRED ON SIDEWALKS THAT ARE 5 FEET OR LESS IN WIDTH. ONE LONGITUDINAL JOINT MARKING WILL BE REQUIRED ON SIDEWALKS OVER 5 FEET BUT LESS THAN 9 FEET IN WIDTH. TWO LONGITUDINAL JOINT MARKINGS WILL BE REQUIRED ON SIDEWALKS OVER 9 FEET BUT LESS THAN 12 FEET IN WIDTH.
- (J) TRANSVERSE JOINT MARKERS ARE TO BE MADE TO FORM BLOCKS AS NEARLY TO SQUARE AS PRACTICAL.
- (K) WHEN LEAVING A SQUARE OPENING IN THE SIDEWALK, THE LENGTH OF THE SIDE OF THE SQUARE OPENING SHOULD BE EQUAL TO THE DIAMETER OF THE FIXED OBJECT PLUS SIXTEEN INCHES. IT WILL BE BORDERED BY HALF INCH EXPANSION JOINT.
- (L) WHEN NEW SIDEWALK IS PLACED ADJACENT TO EXISTING SIDEWALK THE CONTRACTOR SHALL CORRECT ALL ABRUPT CHANGES AND SLOPES TO PROVIDE A SMOOTH TRANSITION FROM THE LIMIT OF CONSTRUCTION TO EXISTING PEDESTRIAN FACILITY.
- (M) DIVIDE THE SURFACE OF SIDEWALKS INTO BLOCKS USING A GROOVING TOOL. SPACE THE GROOVES APPROXIMATELY 5 FEET APART TO PRODUCE SQUARE BLOCKS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- (N) SHAPE AND COMPACT THE SUBGRADE TO A FIRM, EVEN SURFACE IN REASONABLY CLOSE CONFORMITY WITH THE GRADE AND CROSS-SECTION SHOWN ON THE PLANS. REMOVE ALL SOFT AND YIELDING MATERIAL, REPLACE IT WITH ACCEPTABLE MATERIAL, AND COMPACT IT AS DIRECTED BY THE ENGINEER.

(Replaced Std Dwg RP-S-7)

STATE OF TENNESSEE
STANDARD
DRAWING
DEPARTMENT OF TRANSPORTATION

DETAILS
FOR
CONCRETE
SIDEWALK

01-07-2019

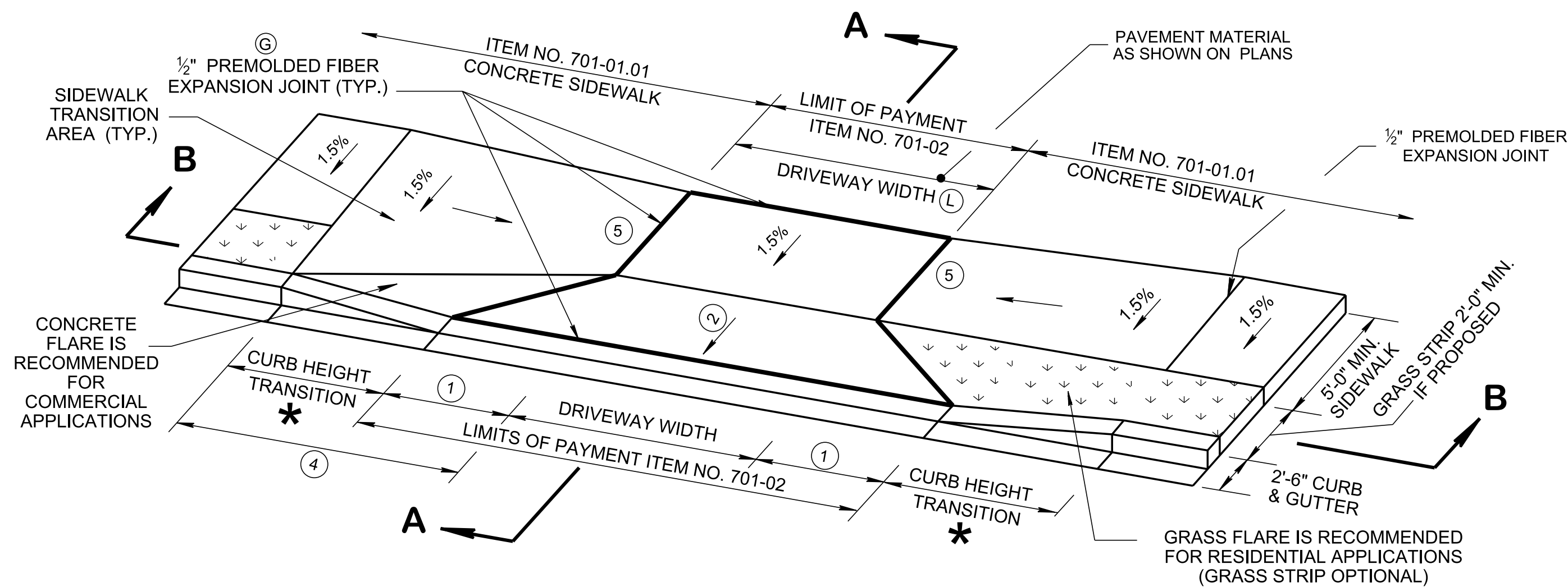
MM-SW-1

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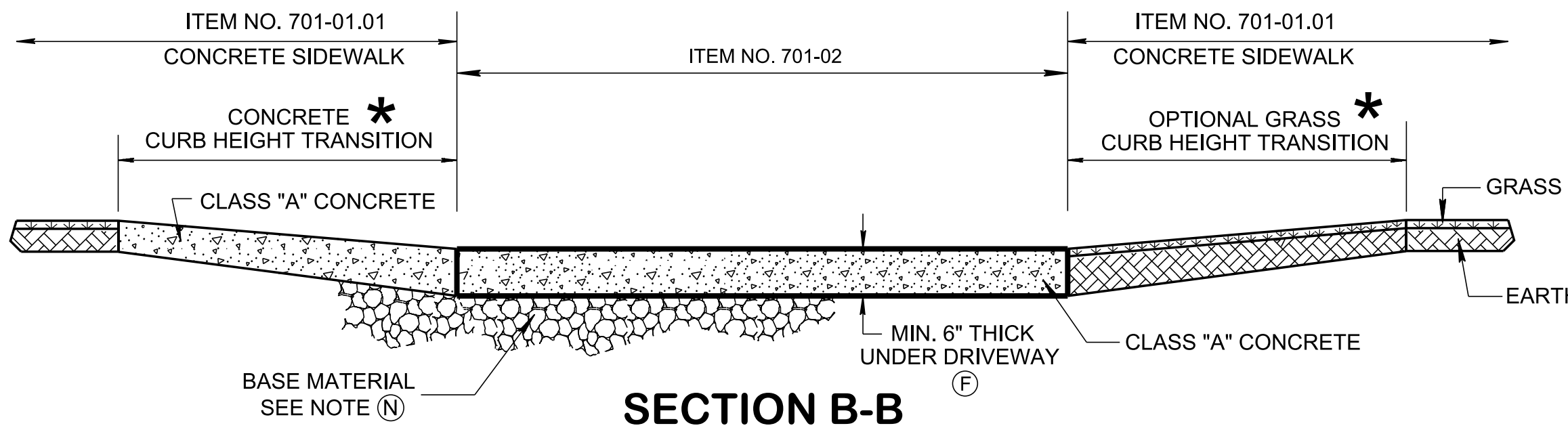
REV. 06-28-19: REVISED GENERAL NOTES (B), (D), (G) & (H) ALONG WITH DETAIL NOTES FOR "TYPICAL SIDEWALK CROSS SECTION WITH GRASS STRIP AND SERVICE APPURTENANCES" AND "SIDEWALK CONSTRUCTION DETAILS WITHOUT GRASS STRIP". ADDED NOTE TO MAILBOX DETAIL.

REV. 03-01-2023: SIDEWALK PLAN VIEWS AND GROOVE JOINT DETAIL WERE ADDED. REMOVED GENERAL NOTE (E), AND ADDED GENERAL NOTES (M) AND (N) SIDEWALK CONSTRUCTION DETAIL WAS REMOVED. SECTION C-C NOTE WAS REVISED.

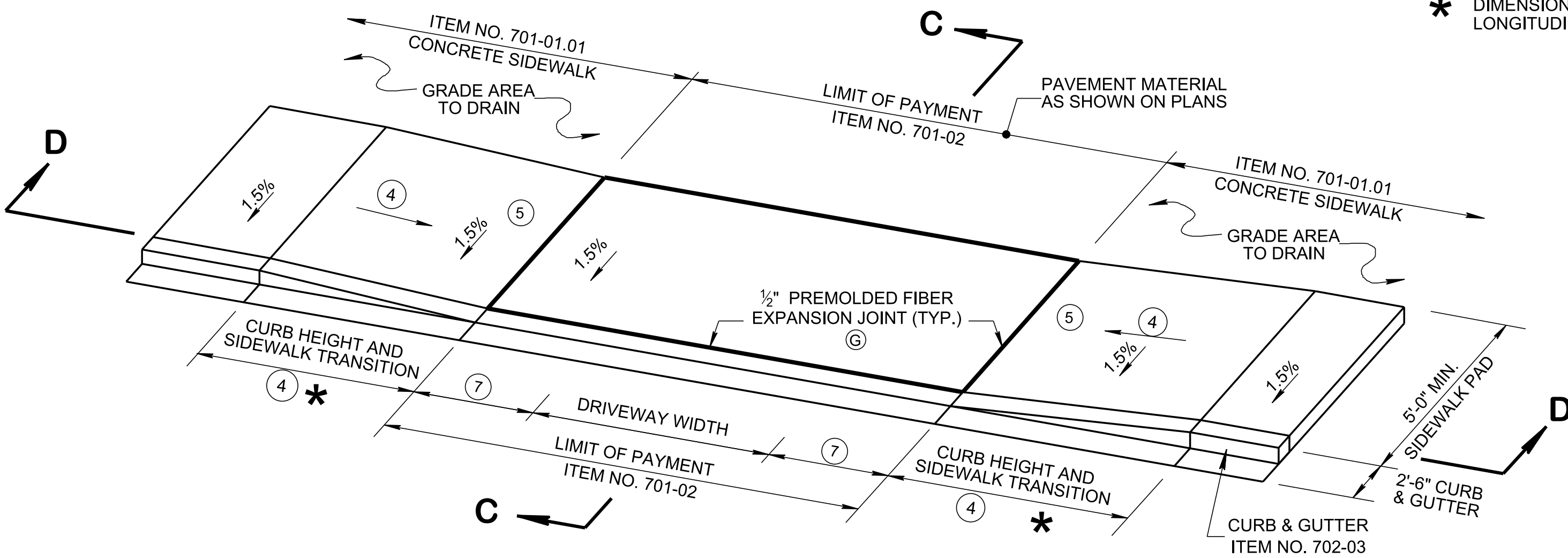
REV. 07-07-2023: REVISED MAILBOX DETAIL AND NOTE.



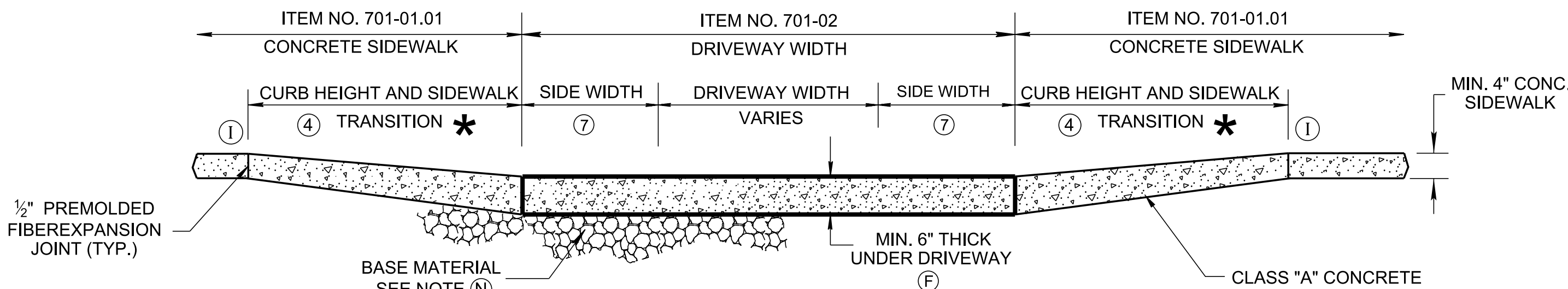
TYPE "B" DRIVEWAY ACROSS LOWERED SIDEWALK
(WITH GRASS STRIP)



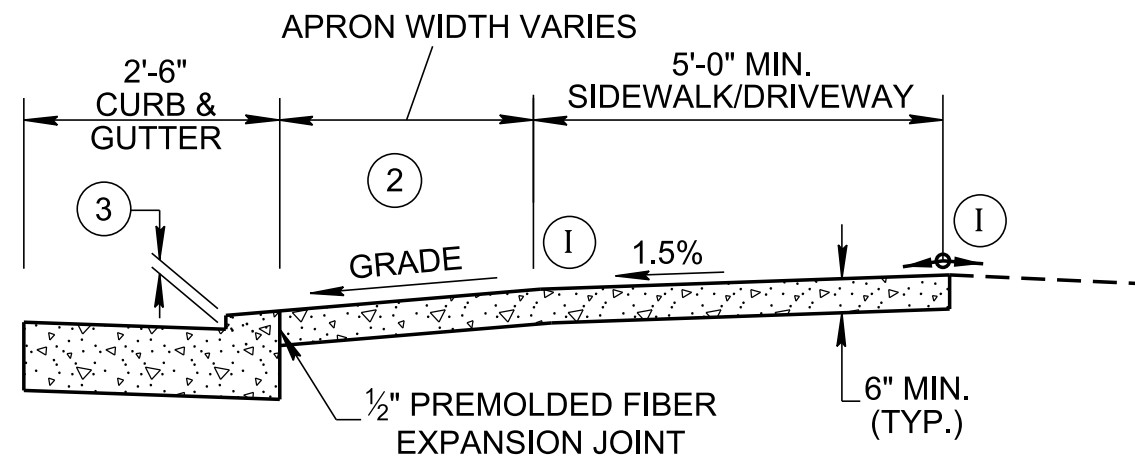
SECTION B-B



TYPE "C" DRIVEWAY ACROSS LOWERED SIDEWALK



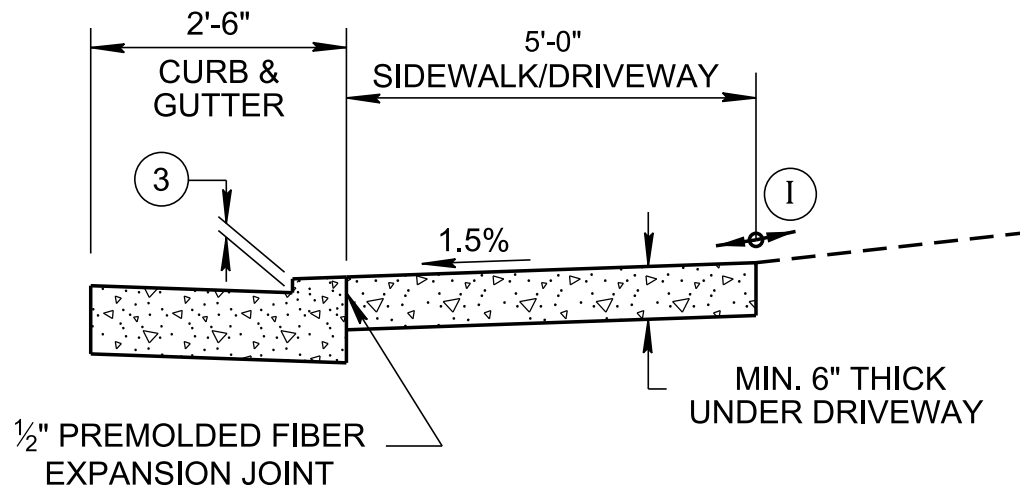
SECTION D-D



SECTION A-A

FOOTNOTES

- SIDE FLARE WIDTH SHOULD BE A MINIMUM 7'-0" FOR COMMERCIAL DRIVEWAYS. SIDE FLARE WIDTH SHOULD BE A MINIMUM 3'-6" FOR RESIDENTIAL DRIVEWAYS.
- DRIVEWAY RAMP GRADE VARIES, 15% MAX. (10% RECOMMENDED) APRON GRADE FOR RESIDENTIAL DRIVEWAYS. 8% MAX. (5% RECOMMENDED) APRON GRADE FOR COMMERCIAL DRIVEWAYS.
- HEIGHT OF LOWERED CURB SHALL BE 2.25 INCHES. SEE STD DWG RP-VC-10 & RP-VC-11.
- THE SLOPE OF THE SIDEWALK AND/OR CURB HEIGHT TRANSITION VARIES TO A MAXIMUM OF 8.33% LENGTH OF TRANSITION IS RELATIVE TO THE LONGITUDINAL ROADWAY GRADE.
- COMMERCIAL DRIVEWAY ENTRANCE TYPICALLY (MAX. 40' WIDE) MAY REQUIRE DETECTABLE WARNING SURFACES IF ENTRANCE SERVES MORE THAN 400 VEHICLES PER DAY. SEE STD. DWG. NOS. MM-CR- SERIES FOR DETAILS.
- 3R PROJECTS MAY REQUIRE SLOPE CORRECTION, PARALLEL CROSS-WALK MARKINGS (ESPECIALLY AT TWO WAY DRIVEWAY ENTRANCES), AND DETECTABLE DOME SURFACE TO MAINTAIN CONTINUITY AT COMMERCIAL DRIVE ENTRANCES. ADDITIONAL SIGNS (WATCH FOR PED) MAY BE ADDED AT DRIVEWAYS BY THE DIRECTION OF AN ENGINEER IF NEEDED.
- SIDE WIDTH SHOULD BE A MINIMUM 7'-0" FOR COMMERCIAL DRIVEWAYS. SIDE WIDTH SHOULD BE A MINIMUM 3'-6" FOR RESIDENTIAL DRIVEWAYS.



SECTION C-C

LEGEND

- * DIMENSION VARIES RELATIVE TO LONGITUDINAL ROADWAY GRADE.

GENERAL NOTES

- DUE TO THE ELEVATION CHANGE FOR PEDESTRIANS ON THE SIDEWALK, THIS APPLICATION IS UNDESIRABLE AND IS TO BE USED IN LIMITED APPLICATIONS. SEE RP-D-15 FOR THE PREFERRED DRIVEWAY TYPE.
- 5'-0" MINIMUM SIDEWALK WITH A MAXIMUM CROSS SLOPE OF 1.5% THROUGH DRIVEWAYS.
- DESIGNER TO CHECK GUTTER FLOW DEPTH AT DRIVEWAY LOCATIONS TO ASSURE THAT THE DESIGN FLOW DOES NOT OVERTOP THE SIDEWALK AREA. IF OVERTOPPING OCCURS, PLACE AN INLET AT THE UPSTREAM SIDE OF THE DRIVEWAY OR PERFORM OTHER DESIGN MITIGATION
- THE SLOPE OF THE LANDING AREA SHALL NOT EXCEED 1.5% IN THE SIDEWALK AREA.
- DRIVEWAYS TO BE BUILT COMPLETE OR IN PART AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- ALL CONCRETE DRIVEWAYS TO BE 6" UNIFORM THICKNESS, UNLESS OTHERWISE SHOWN ON PLANS.
- EXPANSION JOINTS ARE TO BE PLACED AS INDICATED ON THE PLANS. WHEN THE BACK OF THE DRIVEWAY ABUTS AGAINST A CONCRETE DRIVEWAY OR BUILDING, AN ADDITIONAL EXPANSION JOINT WILL BE PLACED AT THAT LOCATION.
- THE ROADWAY DESIGNER SHALL CONSIDER THE USE OF A CATCH BASIN ON EITHER SIDE OF DRIVEWAY. CAREFUL CONSIDERATION TO THE PLACEMENT OF CATCH BASINS SHALL BE TAKEN IF THE DRIVEWAY IS IN A VERTICAL SAG CURVE.
- ALGEBRAIC DIFFERENCE NOT TO EXCEED 10.0%.
- PAY ITEMS:

ITEM NO:	303-01,	MINERAL AGGREGATE, TYPE A BASE, GRADING D,	PER TON.
ITEM NO:	701-01.01,	CONCRETE SIDEWALK (4"),	PER S.F.
ITEM NO:	701-02,	CONCRETE DRIVEWAY,	PER S.F.
ITEM NO:	701-02.02,	CONCRETE DRIVEWAY (8"),	PER S.F.
ITEM NO:	702-03,	CONCRETE COMBINED CURB & GUTTER,	PER C.Y.
- WHEN MORE THAN 2 DRIVEWAYS ARE PROPOSED, USE TYPE "A" DRIVEWAY AS SHOWN ON STANDARD DRAWING RP-D-15 TO REDUCE ROLLER COASTER EFFECT FOR PEDESTRIANS.
- TYPICAL DRIVEWAY WIDTHS ARE 12' (14' TWO WAY) FOR RESIDENTIAL AND 24' (40' MAX.) FOR COMMERCIAL. REFER TO SECTION 5 (ACCESS DESIGN) IN THE MANUAL FOR CONSTRUCTING DRIVEWAY ENTRANCES ON STATE HIGHWAYS (2015).
- ALL SIDEWALKS SHALL HAVE A MINIMUM CONCRETE THICKNESS OF 4". THE SIDEWALK TRANSITION THICKNESS IS DEPEND ON THE DRIVEWAY AND THE SIDEWALK THICKNESSES, THE COST OF THE SIDEWALK TRANSITION WILL BE INCLUDED IN THE PAY ITEM NO. OF 701-01.01.
- MINIMUM 4" MINERAL AGGREGATE BASE MATERIAL ITEM NO. 303-01 SHALL BE INSTALLED UNDER NEW CONCRETE DRIVEWAYS. SITE SPECIFIC PAVEMENT DESIGN MAY BE REQUIRED FOR COMMERCIAL DRIVEWAYS USED AS A DELIVERY ACCESS AS WELL. A DRIVEWAY PAVEMENT DESIGN WITH 6" CONCRETE PAVEMENT AND 4" AGGREGATE DEPTH MAY BE LIMITED TO LIGHT COMMERCIAL VEHICULAR TRAFFIC.

APPROVED BY FHWA
(ALL OTHERS APPROVED BY TDOT)

STATE OF TENNESSEE
STANDARD
DRAWING
DEPARTMENT OF TRANSPORTATION

DETAILS OF
LOWERED
STANDARD
CONCRETE
DRIVEWAYS



* DIMENSION VARIES RELATIVE TO LONGITUDINAL ROADWAY GRADE.



NOT TO SCALE

02-15-2007	RP-D-15
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(A) THIS TYPE OF DRIVEWAY IS PREFERRED OVER THE LOWERED TYPE AS SHOWN ON RP-16 BECAUSE THE ELEVATION OF THE SIDEWALK REMAINS A CONSTANT FOR PEDESTRIANS.

(B) 5'-0" MINIMUM SIDEWALK WITH A MAXIMUM CROSS SLOPE OF 1.5% THROUGH DRIVEWAYS.

(C) DESIGNER TO CHECK GUTTER FLOW DEPTH AT DRIVEWAY LOCATIONS TO ASSURE THAT THE DESIGN FLOW DOES NOT OVERTOP THE SIDEWALK AREA. IF OVERTOPPING OCCURS, PLACE AN INLET AT THE UPSTREAM SIDE OF THE DRIVEWAY OR PERFORM OTHER DESIGN MITIGATION.

(D) THE SLOPE OF THE LANDING AREA SHALL NOT EXCEED 1.5% IN THE SIDEWALK AREA.

(E) DRIVEWAYS TO BE BUILT COMPLETE OR IN PART AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

(F) ALL DRIVEWAYS TO BE 6" UNIFORM THICKNESS, UNLESS OTHERWISE SHOWN ON PLANS.

(G) EXPANSION JOINTS ARE TO BE PLACED AS INDICATED ON THE PLANS. WHEN THE BACK OF THE DRIVEWAY ABUTS AGAINST A CONCRETE DRIVEWAY OR BUILDING, AN ADDITIONAL EXPANSION JOINT WILL BE PLACED AT THAT LOCATION.

(H) THE ROADWAY DESIGNER SHALL CONSIDER THE USE OF A CATCH BASIN ON EITHER SIDE OF THE DRIVEWAY. CAREFUL CONSIDERATION TO THE PLACEMENT OF CATCH BASINS SHALL BE TAKEN IF THE DRIVEWAY IS IN A VERTICAL SAG CURVE.

(I) PAY ITEMS:

ITEM NO:	303-01,	MINERAL AGGREGATE, TYPE A BASE, GRADING D,	PER TON.
ITEM NO:	701-01.01,	CONCRETE SIDEWALK (4"),	PER S.F.
ITEM NO:	701-02,	CONCRETE DRIVEWAY,	PER S.F.
ITEM NO:	701-02.02,	CONCRETE DRIVEWAY (8"),	PER S.F.
ITEM NO:	702-03,	CONCRETE COMBINED CURB AND GUTTER,	PER C.Y.

(J) TYPICAL DRIVEWAY WIDTHS ARE 12' (14' TWO WAY) FOR RESIDENTIAL AND 24' (40' MAX.) FOR COMMERCIAL. REFER TO SECTION 5 (ACCESS DESIGN) IN THE MANUAL FOR CONSTRUCTING DRIVEWAY ENTRANCES ON STATE HIGHWAY (2015).

(K) ALL SIDEWAYS SHALL HAVE A MINIMUM CONCRETE THICKNESS OF 4". THE SIDEWALK TRANSITION THICKNESS IS DEPENDENT UPON THE DRIVEWAY AND SIDEWALK THICKNESSES, THE COST OF THE SIDEWALK TRANSITION WILL BE INCLUDED IN THE PAY ITEM NO. OF 701-01.01.

(L) MINIMUM 4" MINERAL AGGREGATE BASE MATERIAL ITEM NO. 303-01 SHALL BE INSTALLED UNDER NEW CONCRETE DRIVEWAYS. SITE SPECIFIC PAVEMENT DESIGN MAY BE REQUIRED FOR COMMERCIAL DRIVEWAYS USED AS A DELIVERY ACCESS AS WELL. A DRIVEWAY PAVEMENT DESIGN WITH 6" CONCRETE PAVEMENT AND 4" AGGREGATE DEPTH MAY BE LIMITED TO LIGHT COMMERCIAL VEHICULAR TRAFFIC.

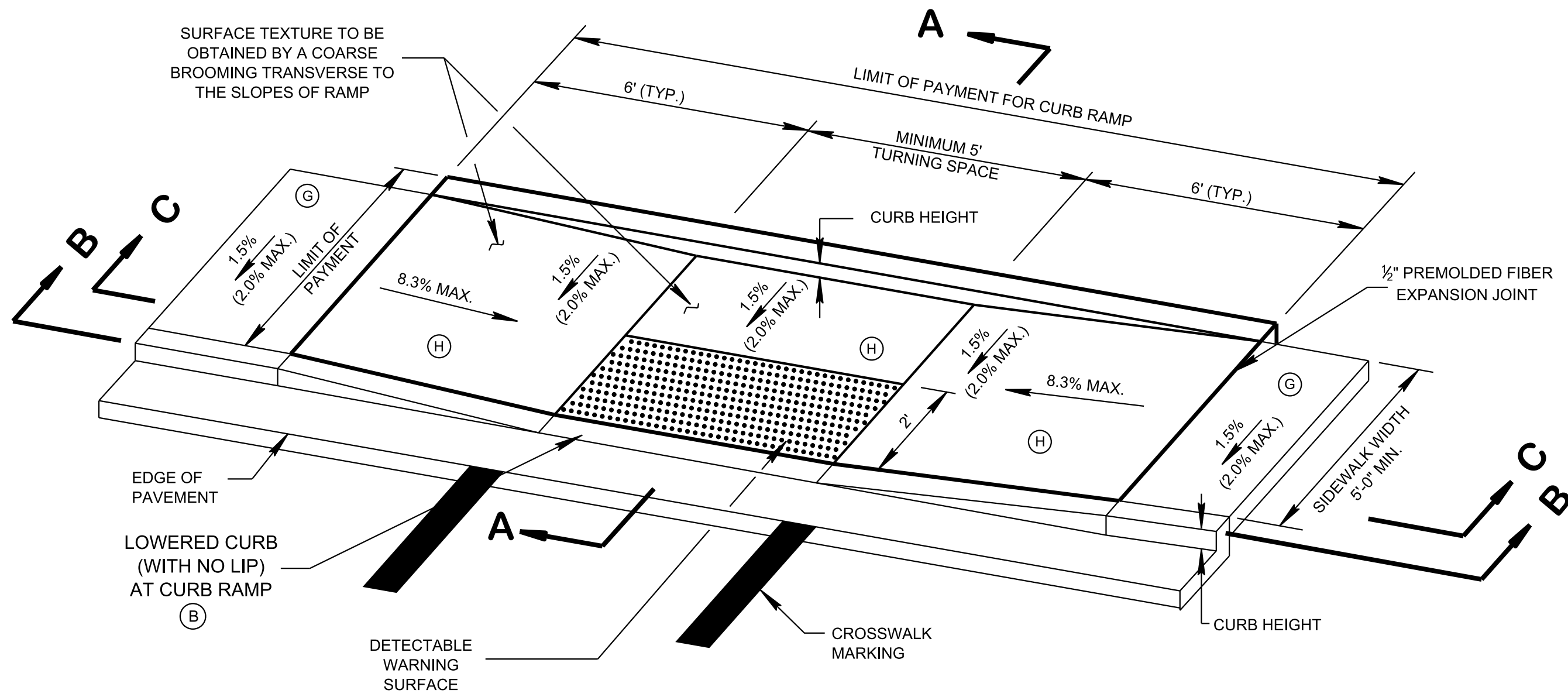


- MINOR REVISION -- FHWA
APPROVAL NOT REQUIRED**

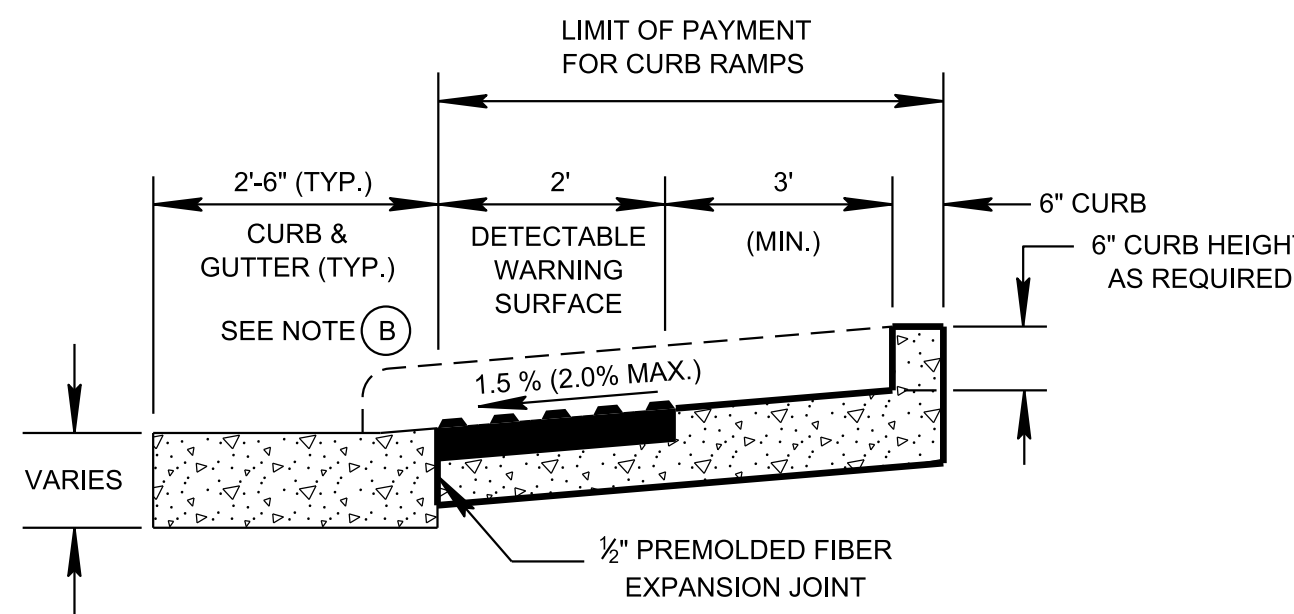
**STATE OF TENNESSEE
DEPARTMENT OF
TRANSPORTATION**

PERPENDICULAR CURB RAMP

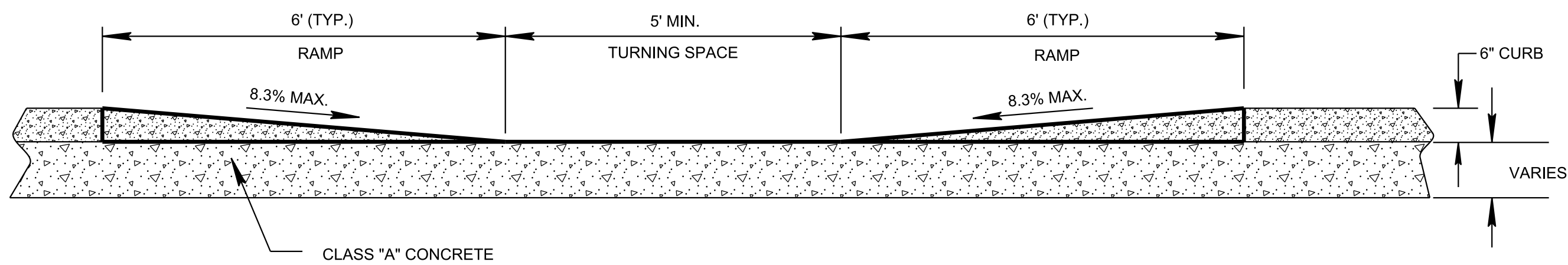
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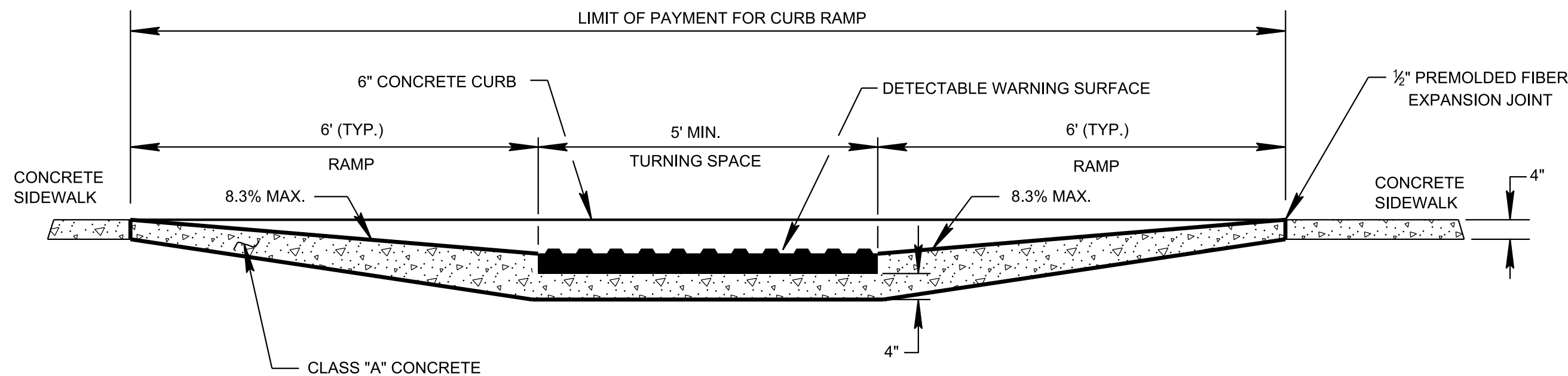
PARALLEL CURB RAMP DETAIL
DIMENSIONS SHOWN ABOVE FOR 0% LONGITUDINAL ROADWAY GRADE



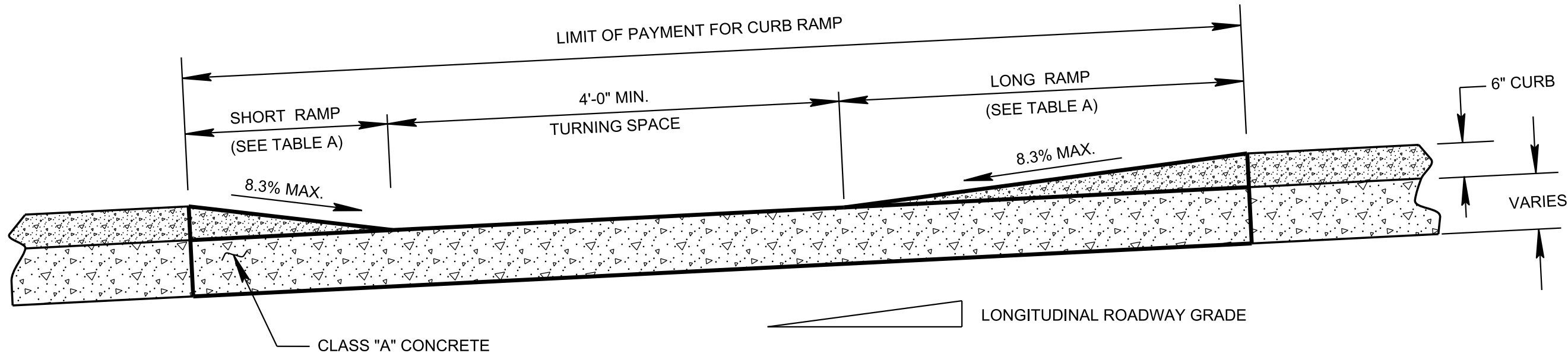
SECTION A-A



SECTION B-B
DIMENSIONS SHOWN ABOVE FOR 0% LONGITUDINAL ROADWAY GRADE



SECTION C-C
DIMENSIONS SHOWN ABOVE FOR 0% LONGITUDINAL ROADWAY GRADE



ALTERNATE SECTION B-B
PARALLEL CURB RAMP DETAIL SHOWN WITH LONGITUDINAL ROADWAY GRADE
NOTE: MODIFICATIONS MAY BE REQUIRED FOR LONGITUDINAL ROADWAY GRADES STEEPER THAN 5%. ENGINEER SHOULD BE NOTIFIED FOR ASSESSMENT IF THE CURB RAMP RUN EXCEEDS 15' IN LENGTH DUE TO THE LONGITUDINAL ROADWAY GRADE.

TABLE A

LONGITUDINAL ROADWAY GRADE	LONG RAMP	SHORT RAMP	PAY AREA S.F.
5 %	15'-0"	3'-9"	114
4 %	11'-6"	4'-1"	98
3 %	9'-5"	4'-5"	90
2 %	7'-11"	4'-10"	84
1 %	6'-10"	5'-5"	92
0 %	6'-0"	6'-0"	80

BASED ON 5' SIDEWALK WIDTH (EXCLUDING BACK CURB)

GENERAL NOTES

- (A) PARALLEL CURB RAMP(S) MAY BE USE WHEN SIDEWALK WIDTH IS LESS THAN 12' AND CURB RAMP IS LOCATED ALONG TANGENT SIDEWALK SECTION OF ROADWAY. FOR PARALLEL CURB RAMP(S) INSTALLED IN CURVE SEE STD. DWG. RP-H-9.
- (B) CURB SHALL BE FLUSH ACROSS ENTIRE WIDTH OF CURB RAMP. DETECTABLE WARNING SURFACES SHALL EXTEND 2' IN THE DIRECTION OF PEDESTRIAN TRAVEL. AT CURB RAMP(S) AND BLENDED TRANSITIONS, DETECTABLE WARNING SURFACES SHALL EXTEND THE FULL WIDTH OF THE RAMP RUN (EXCLUDING ANY FLARED SIDES), BLENDED TRANSITION, OR TURNING SPACE. SEE RP-H-3 FOR DETECTABLE WARNING SURFACE DETAILS.
- (C) DESIGN / CONSTRUCTION MODIFICATIONS MAY BE REQUIRED FOR CURB RAMP(S) TO BE INSTALLED ALONG A ROADWAY WITH LONGITUDINAL GRADES EXCEEDING 5%. ENGINEER SHOULD BE NOTIFIED FOR ASSESSMENT IF THE CURB RAMP SIDE FLARES EXCEED 15' IN LENGTH DUE TO THE LONGITUDINAL GRADE.
- (D) PAYMENT:
- NEW:
ALL COSTS OF INSTALLING CURB RAMP(S), INCLUDING DETECTABLE WARNING SURFACE(S) IN NEWLY CONSTRUCTED SIDEWALK AREAS, SHALL BE PAID BY ITEM NO. 701-02.03, CONCRETE CURB RAMP, PER SQUARE FOOT.
- PAYMENT SHALL INCLUDE ALL MATERIALS, EQUIPMENT, AND LABOR NECESSARY FOR CONSTRUCTION OF THE CURB RAMP(S), INCLUDING INSTALLATION OF DETECTABLE WARNING SURFACE(S).
- RETROFIT:
ALL COSTS OF INSTALLING CURB RAMP(S), INCLUDING DETECTABLE WARNING SURFACE(S) IN EXISTING SIDEWALK AREAS, REMOVAL OF THE EXISTING SIDEWALK, AND ADJUSTMENT OF GUTTER PAN SLOPE, SHALL BE PAID BY ITEM NO. 701-02.01, CONCRETE CURB RAMP (RETROFIT), PER SQUARE FOOT.
- PAYMENT SHALL INCLUDE ALL MATERIALS, EQUIPMENT, AND LABOR INSTALLATION OF CURB RAMP(S), INCLUDING INSTALLATION OF DETECTABLE WARNING SURFACE(S).
- COST OF CURB AND GUTTER TO BE INCLUDED IN THE PRICE OF ITEM NO. 702-01, CONCRETE CURB, PER C. Y. OR ITEM NO. 702-03, CONCRETE COMBINED CURB & GUTTER, PER C. Y.
- (E) FOR SIGNALIZED INTERSECTIONS THAT REQUIRE PEDESTRIAN SIGNAL PUSHBUTTONS, SEE TDOT TRAFFIC DESIGN MANUAL FOR PLACEMENT DETAILS.
- (F) FOR ADDITIONAL SIDEWALK DETAILS AND IF MAILBOXES ARE REMOVED DURING INSTALLATION OF THE CURB RAMP, PROVIDE A 12" X 12" OPENING BEHIND THE CURB. SEE STD. DWG. RP-S-7.
- (G) WHERE NEW CURB RAMP CONDITIONS DO NOT MEET EXISTING SIDEWALK, THE DESIGNER SHALL ADD ADDITIONAL QUANTITY FOR UP TO 15 FEET IN EACH DIRECTION OF SIDEWALK MODIFICATION TO TIE TO THE EXISTING GRADE.
- (H) SURFACE TEXTURE TO BE OBTAINED BY A COARSE BROOMING TRANSVERSE TO THE SLOPE OF CURB RAMP.
- (I) SEE STD. DWG. T-M-4 FOR CROSSWALK MARKING DETAILS.

- REV. 4-13-11: ADDED FOOTNOTE (I) ADJUSTED DIMENSIONS IN RAMP DETAIL, MISC. EDITS TO DRAWING.
- REV. 1-15-13: MODIFIED LANDING WIDTH AND REVISED NOTES.
- REV. 6-4-13: CHANGED CROSS SLOPE TO 1.5 % DESIRABLE. ADDED NOTES (G) AND (H). CHANGED ITEM DESCRIPTION.
- REV. 1-30-15: UPDATE NOTES, REMOVED PAVERS. UPDATED TABLE A.
- REV. 10-10-16: REVISED GENERAL NOTES, MISC. REVISIONS TO DETAILS.
- REV. 07-16-18: REVISED GENERAL NOTES AND TABLE A. REDREW SHEET.

MINOR REVISION -- FHWA
APPROVAL NOT REQUIRED

STATE OF TENNESSEE
DEPARTMENT OF
TRANSPORTATION

PARALLEL
CURB RAMP

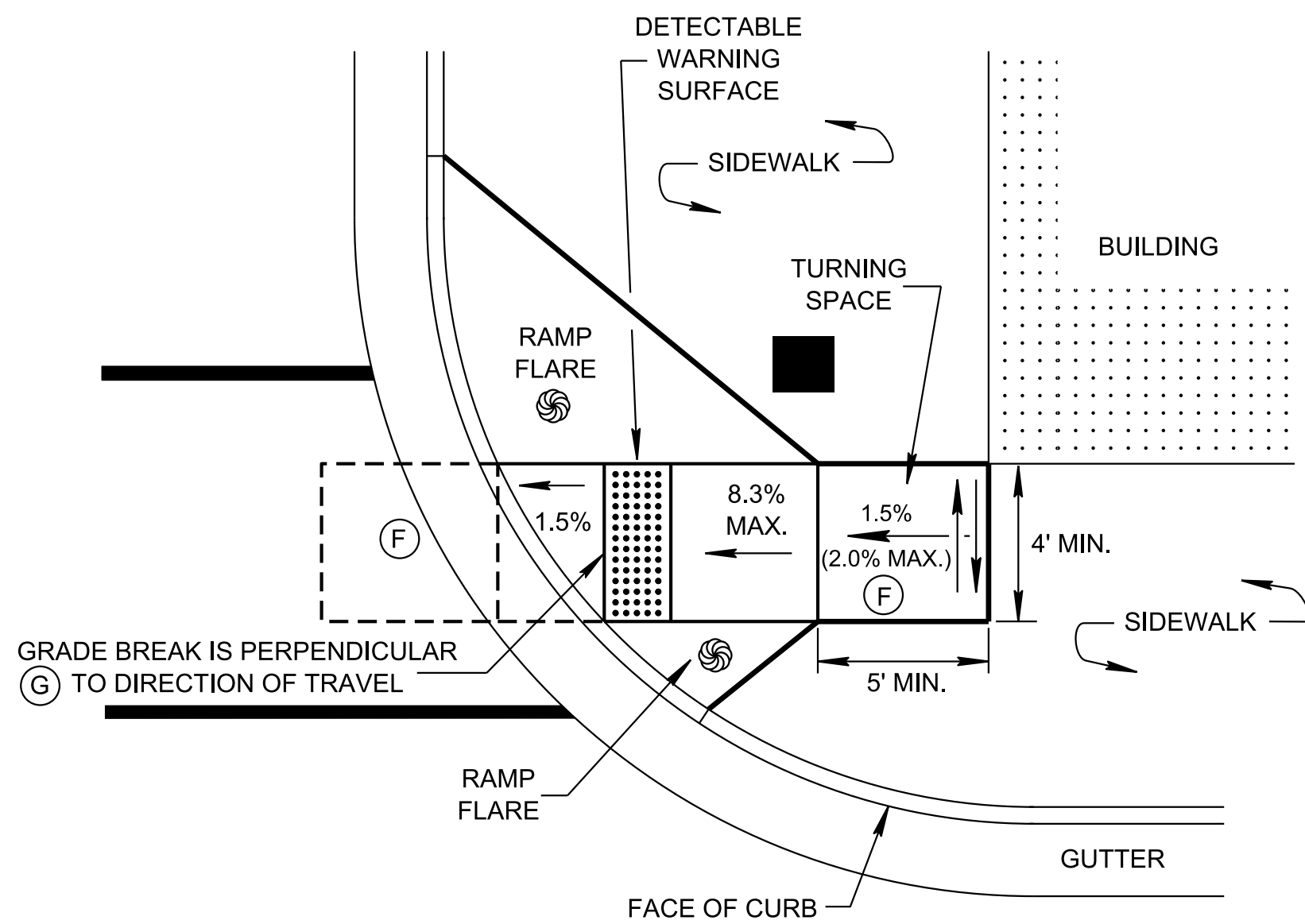
01-15-07

RP-H-5

NOT TO SCALE

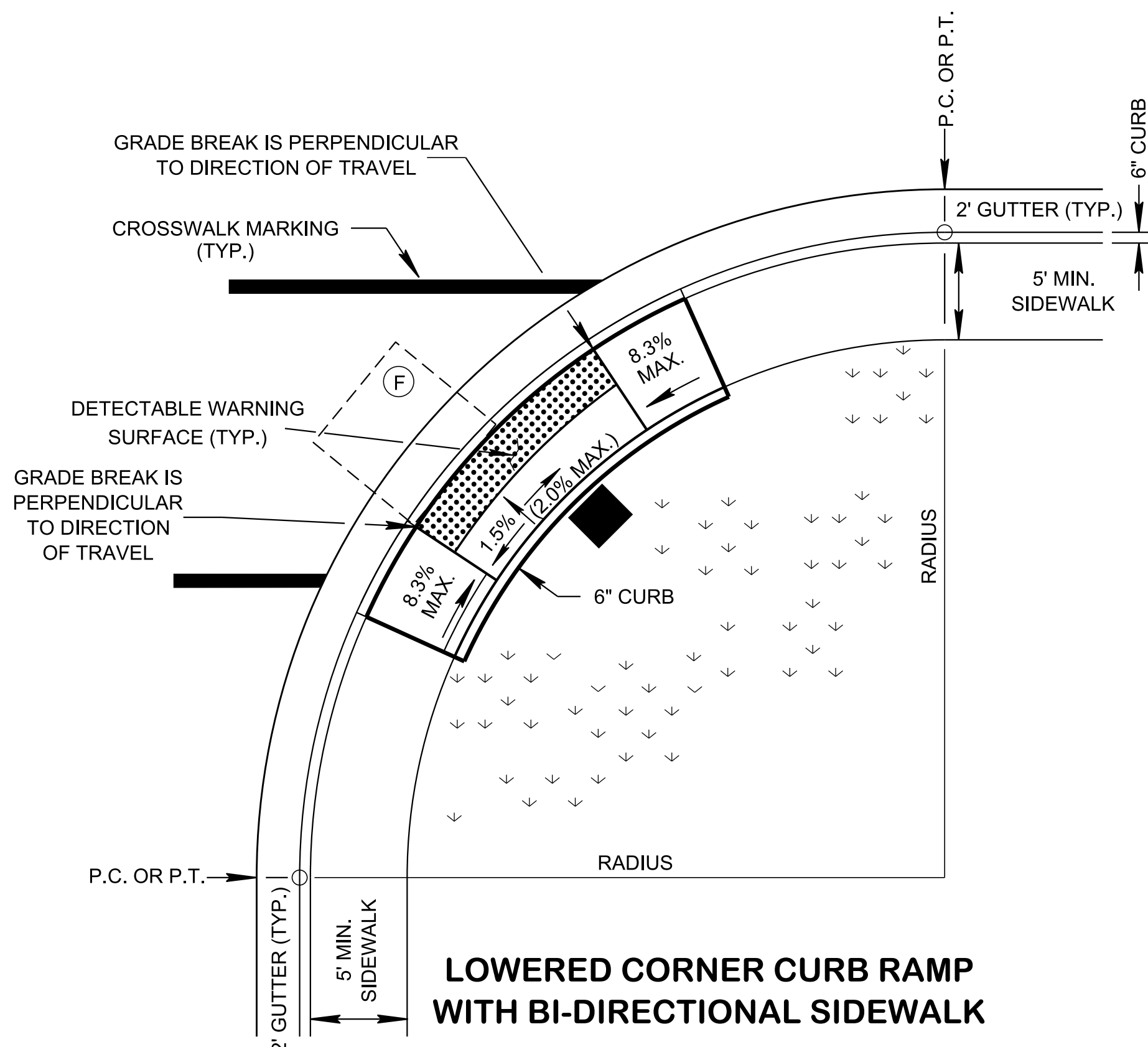
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PERPENDICULAR CURB RAMPS
FOR SINGLE CROSSING

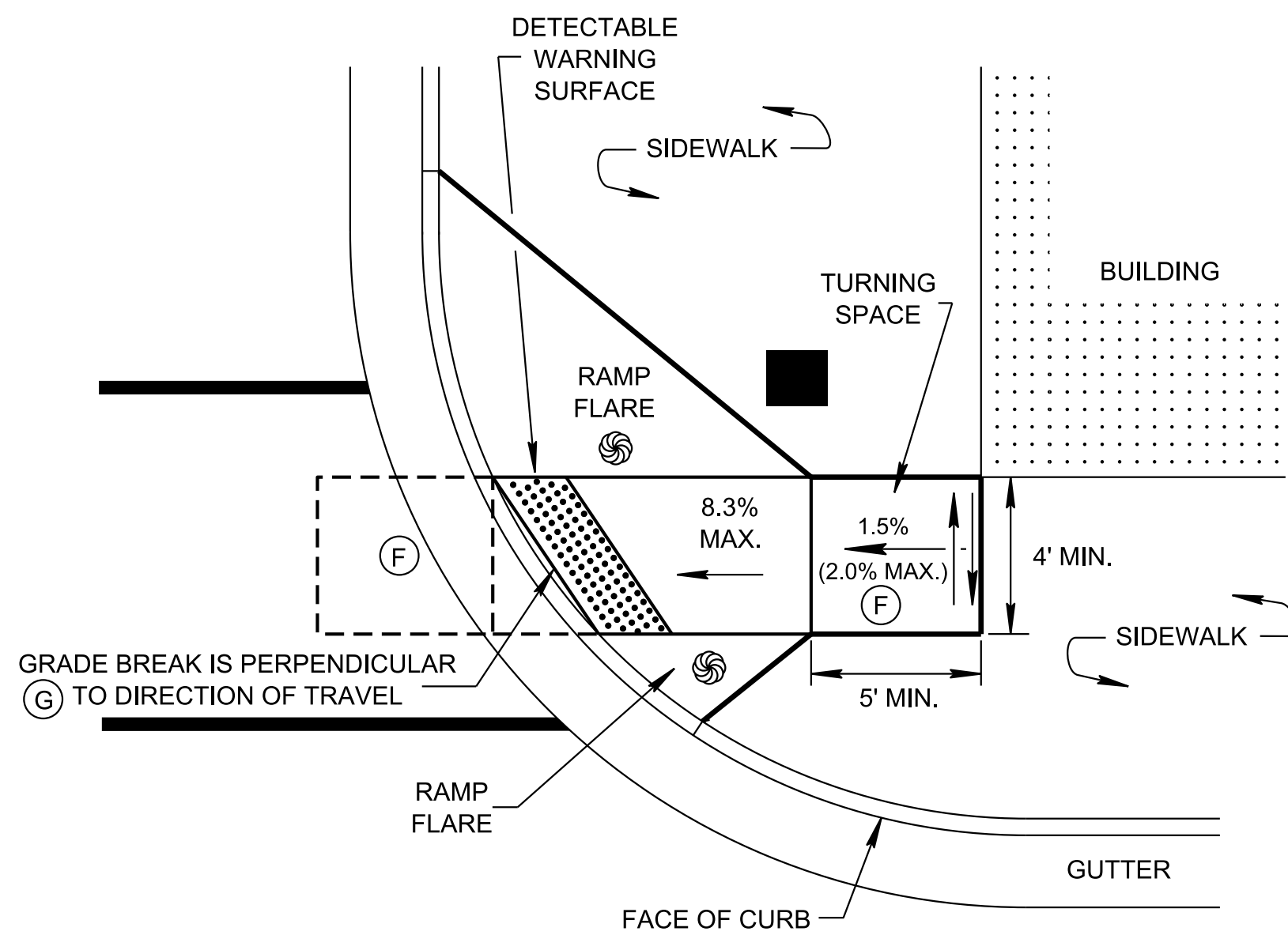


BI-DIRECTIONAL SIDEWALK

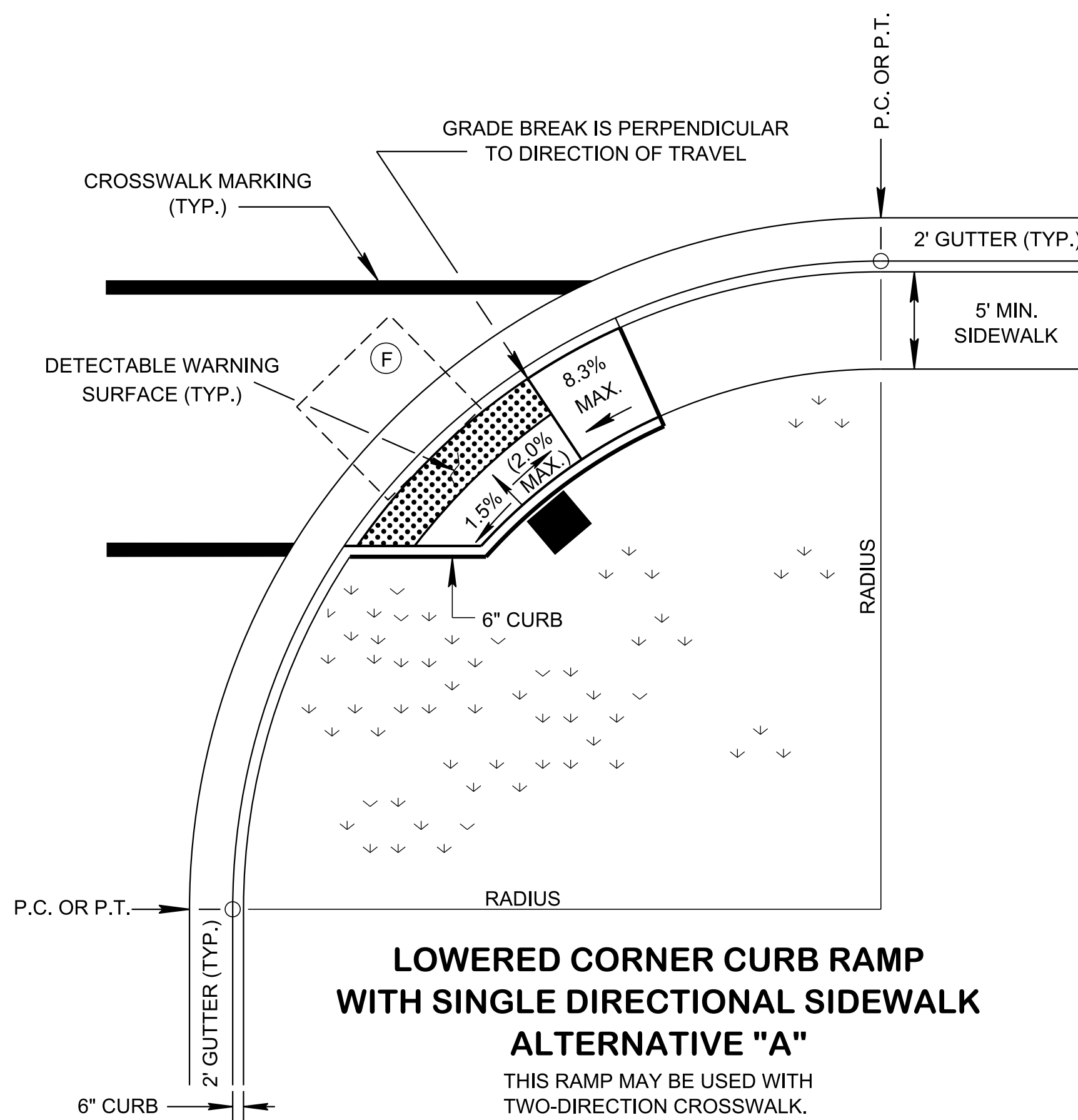
PARALLEL CURB RAMPS
FOR SINGLE CROSSING



LOWERED CORNER CURB RAMP
WITH BI-DIRECTIONAL SIDEWALK



BI-DIRECTIONAL SIDEWALK
ALTERNATIVE PLACEMENT



LOWERED CORNER CURB RAMP
WITH SINGLE DIRECTIONAL SIDEWALK

ALTERNATIVE "A"

THIS RAMP MAY BE USED WITH
TWO-DIRECTION CROSSWALK.

LEGEND



DIMENSION VARIES RELATIVE TO LONGITUDINAL
ROADWAY GRADE, 8.3% DESIRABLE (10.0% MAX.)



DENOTES: APPROXIMATE PEDESTRIAN POLE/PUSHBUTTON
LOCATION FOR SIGNALIZED INTERSECTIONS

GENERAL NOTES

- (A) FOR DETECTABLE WARNING SURFACE DETAILS SEE STD. DWG. RP-H-3. FOR ADDITIONAL DETAILS AND OTHER INFORMATION FOR PERPENDICULAR CURB RAMPS NOT SHOWN ON THIS DRAWING SEE STD. DWG. RP-H-4 AND FOR PARALLEL CURB RAMPS SEE STD. DWG. RP-H-5. FOR CROSSWALK MARKING DETAILS SEE STD. DWG. T-M-4.
- (B) IF PERPENDICULAR CURB RAMPS AND TURNING SPACE CANNOT BE CONSTRUCTED DUE TO LIMITED RIGHT-OF-WAY, USE PARALLEL CURB RAMP INSTEAD.
- (C) CARE SHALL BE TAKEN ON ALL ROADWAY CURB RAMPS AT INTERSECTIONS WITH SIDEWALK AND CURB RAMPS TO ENSURE A UNIFORM GRADE AROUND THEM. THE ROADWAY CURB RAMP GRADE SHALL BE FREE OF SAGS AND SHORT GRADE CHANGES.
- (D) SIDEWALK WIDTH SHALL NOT INCLUDE 6" CONCRETE CURB. THE DESIRABLE SIDEWALK CROSS SLOPE IS 1.5 %, ABSOLUTE MAXIMUM IS 2.0%.
- (E) DRAINAGE STRUCTURES SHALL NOT BE PLACED IN THE CROSSWALK OR IN FRONT OF THE CURB RAMP.
- (F) TURNING SPACE \ CLEAR SPACE:

CLEAR SPACE BEYOND THE BOTTOM GRADE BREAK, A CLEAR SPACE 4' (MIN.) BY 4' (MIN.) SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN STREET CROSSING AND WHOLLY OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE.

TURNING SPACE MUST BE PROVIDED AT THE TOP OF PERPENDICULAR CURB RAMPS. THE TURNING SPACE MUST BE 4' (MIN.) BY 4' (MIN.), AND IS PERMITTED TO OVERLAP OTHER TURNING SPACES AND CLEAR SPACES. WHERE THE TURNING SPACE IS CONSTRAINED AT THE BACK OF THE SIDEWALK, THE TURNING SPACE MUST BE 4' (MIN.) BY 5' (MIN.), WITH THE 5' DIMENSION PROVIDED IN THE DIRECTION OF THE RAMP RUN.

FOR PARALLEL CURB RAMPS, A TURNING SPACE 4' (MIN.) BY 4' (MIN.) SHALL BE PROVIDED AT THE BOTTOM OF THE CURB RAMP AND SHALL BE PERMITTED TO OVERLAP OTHER TURNING SPACES AND CLEAR SPACES. IF THE TURNING SPACE IS CONSTRAINED ON 2 OR MORE SIDES, THE TURNING SPACE SHALL 4' (MIN.) BY 5' (MIN.). THE 5' DIMENSION SHALL BE PROVIDED IN THE DIRECTION OF THE PEDESTRIAN STREET CROSSING.
- (G) GRADE BREAKS:

GRADE BREAKS AT THE TOP AND BOTTOM OF CURB RAMP RUNS SHALL BE PERPENDICULAR TO THE DIRECTION OF THE CURB RAMP RUN. GRADE BREAKS SHALL NOT BE PERMITTED ON THE SURFACE OF CURB RAMP RUNS AND TURNING SPACES. SURFACE SLOPES THAT MEET AT GRADE BREAKS SHALL BE FLUSH.

WHERE THE END OF BOTTOM GRADE BREAK ARE BEHIND THE BACK OF CURB AND THE DISTANCE FROM EITHER END OF THE BOTTOM GRADE BREAK TO THE BACK OF CURB IS 5' OR LESS, DETECTABLE WARNING SURFACES SHALL BE PLACED ON THE CURB RAMP RUN WITHIN ONE DOME SPACING OF THE BOTTOM GRADE BREAK.
- (H) PEDESTRIAN SIGNAL PUSHBUTTON:

WHERE PEDESTRIAN SIGNALS ARE PROVIDED AT PEDESTRIAN STREET CROSSINGS, THEY SHALL INCLUDE ACCESSIBLE PEDESTRIAN SIGNALS AND POLE/PEDESTRIAN PUSHBUTTONS COMPLYING WITH SECTIONS 4E.08 THROUGH 4E.13 OF THE MUTCD. FOR DETAILS OF THE PLACING OF PEDESTRIAN SIGNAL PUSHBUTTONS SEE TDOT TRAFFIC DESIGN MANUAL.

WHEN TWO PEDESTRIAN PUSHBUTTONS ARE ON ONE CORNER THEY WILL BE SEPARATED BY 10 FEET.
- (I) PAYMENT:

COST OF CURB AND GUTTER TO BE INCLUDED IN THE PRICE OF ITEM NO. 702-01, CONCRETE CURB, PER C. Y. OR ITEM NO. 702-03, CONCRETE COMBINED CURB & GUTTER, PER C. Y.

NEW CURB RAMPS:
ALL COSTS OF INSTALLING CURB RAMP(S), INCLUDING DETECTABLE WARNING SURFACE(S) IN NEWLY CONSTRUCTED SIDEWALK AREAS, SHALL BE PAID BY ITEM NO. 701-02.03, CONCRETE CURB RAMP, PER SQUARE FOOT.

PAYMENT SHALL INCLUDE ALL MATERIALS, EQUIPMENT, AND LABOR NECESSARY FOR CONSTRUCTION OF THE CURB RAMP(S), INCLUDING INSTALLATION OF DETECTABLE WARNING SURFACE(S).

CURB RAMPS (RETROFIT):
ALL COSTS OF INSTALLING CURB RAMP(S), INCLUDING DETECTABLE WARNING SURFACE(S) IN EXISTING SIDEWALK AREAS, REMOVAL OF THE EXISTING SIDEWALK, AND ADJUSTMENT OF GUTTER PAN SLOPE, SHALL BE PAID BY ITEM NO. 701-02.01, CONCRETE CURB RAMP (RETROFIT), PER SQUARE FOOT.

PAYMENT SHALL INCLUDE ALL MATERIALS, EQUIPMENT, AND LABOR INSTALLATION OF CURB RAMP(S), INCLUDING INSTALLATION OF DETECTABLE WARNING SURFACE(S).

REV. 4-13-11: REVISED TABLE DIMENSIONS, ADDED NOTE (1), AND ADDED GUTTER TO CROSSWALK INTERSECTION DIMENSION.

REV. 5-8-13: REVISED TITLE FOR TERMINOLOGY.

REV. 6-4-13: REVISED NOTE (C), CHANGED TITLE.

REV. 1-30-15: REMOVED ALTERNATE. ADDED GENERAL NOTE (E), (F), (G), (H), (1) AND (4). ADDED GRADE BREAKS DETAILS.

REV. 10-10-16: REVISED GENERAL NOTES, MISC. REVISIONS TO DETAILS.

REV. 07-16-18: REMOVED THE CURB RAMP DETAIL FOR ACCESS TO PEDESTRIAN REFUGE ISLAND. ADDED SINGLE AND BI-DIRECTIONAL SIDEWALK LABELS. REVISED DETAIL HEADING FOR PERPENDICULAR CURB RAMPS FOR SINGLE CROSSING. ADDED DETAILS FROM STD. DWG. RP-H-10 FOR PARALLEL CURB RAMPS FOR SINGLE CROSSING. REVISED GENERAL NOTES. RENAMED AND REDREW SHEET.

MINOR REVISION -- FHWA
APPROVAL NOT REQUIRED

STATE OF TENNESSEE
DEPARTMENT OF
TRANSPORTATION

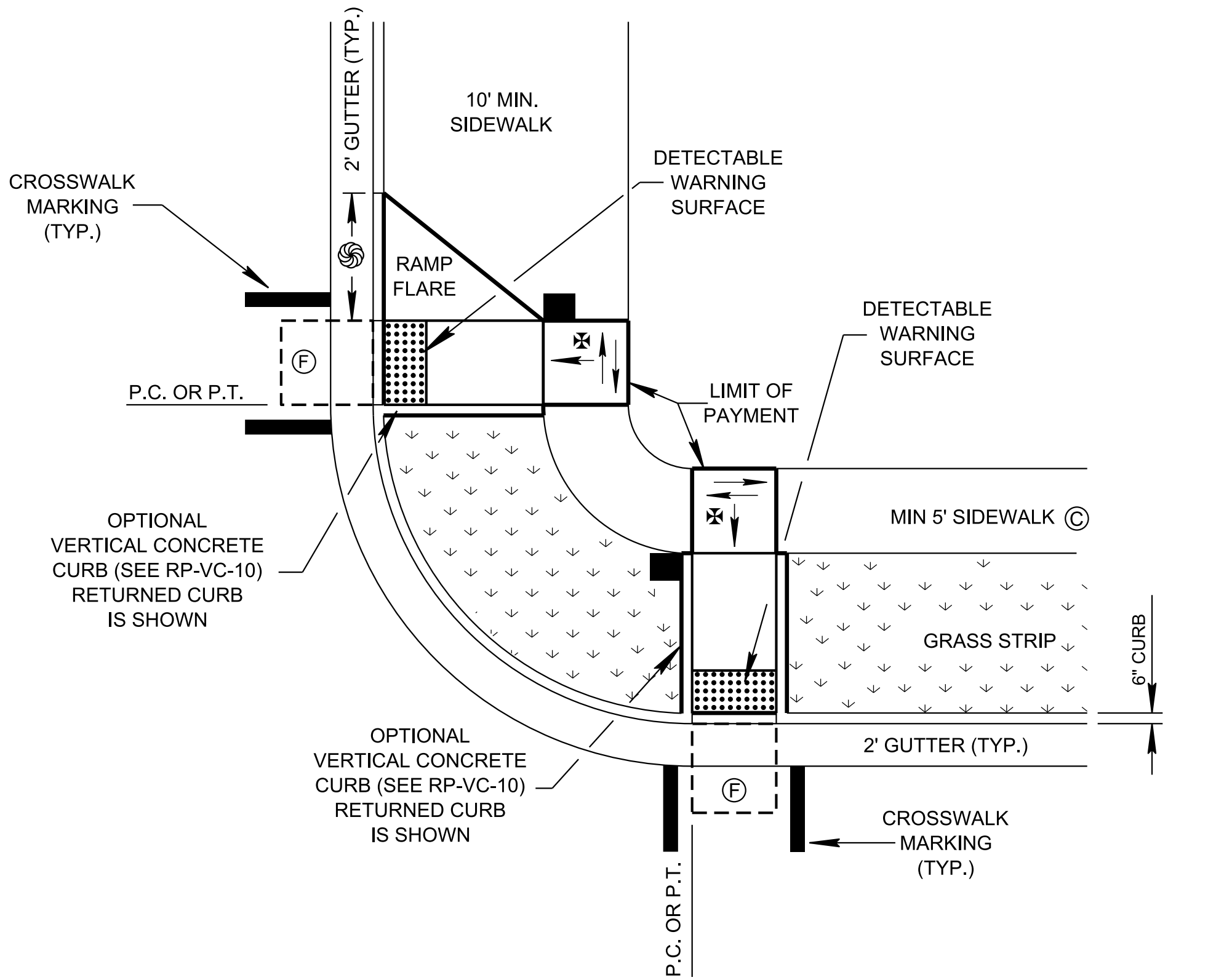
SINGLE CROSSING
CURB RAMP
IN CURVE

01-15-07

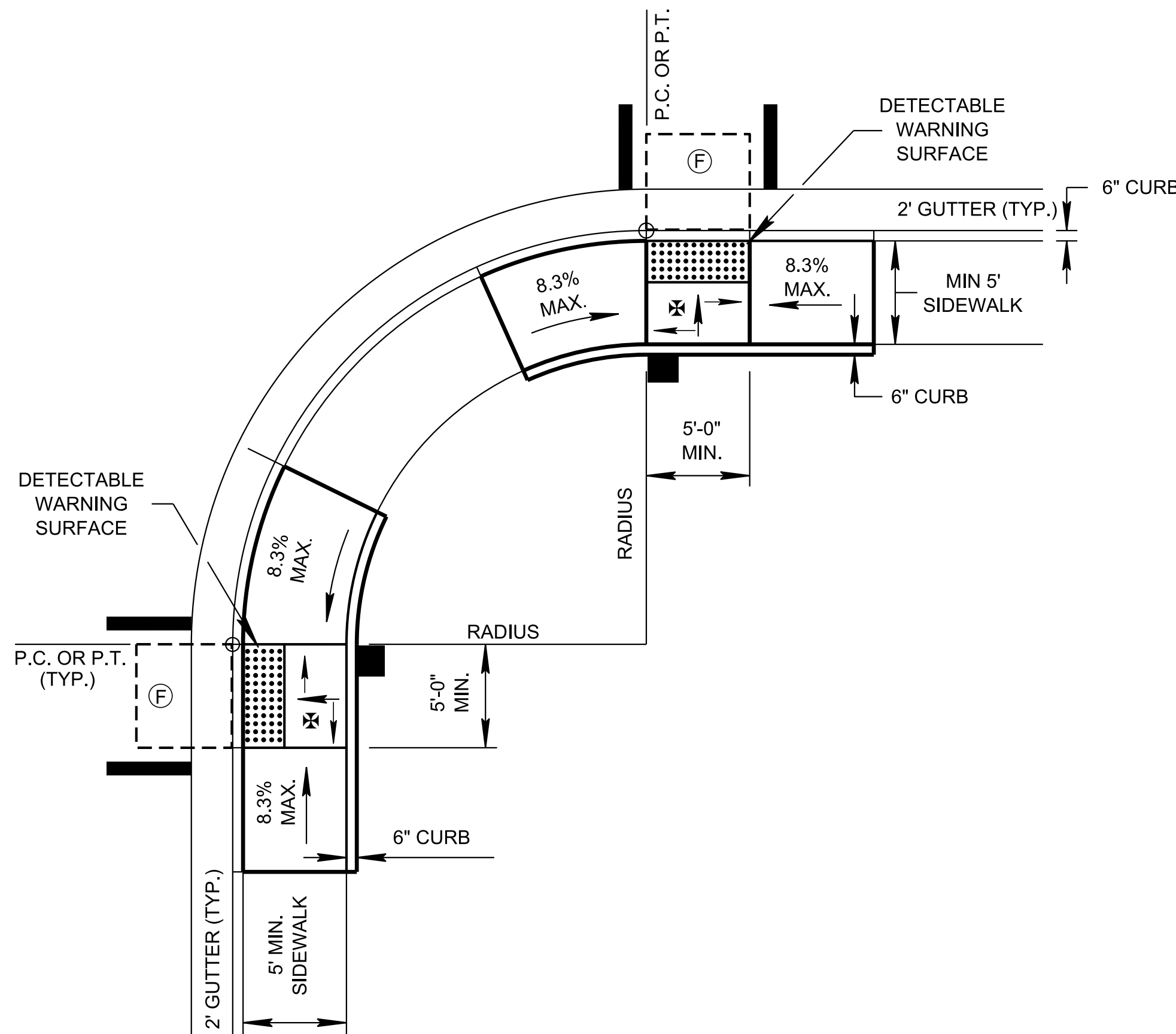
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**PERPENDICULAR CURB RAMP OUTSIDE RADIUS
(WITH GRASS STRIP OR WIDE SIDEWALK)**



**PARALLEL CURB RAMPS
OUTSIDE RADIUS**

LEGEND	
	DIMENSION VARIES RELATIVE TO LONGITUDINAL ROADWAY GRADE, 8.3% DESIRABLE (10.0% MAX.)
	DENOTES: APPROXIMATE PEDESTRIAN POLE/PUSHBUTTON LOCATION FOR SIGNALIZED INTERSECTIONS
	1.5% (2.0% MAX.)

GENERAL NOTES	
(A)	FOR DETECTABLE WARNING SURFACE DETAILS SEE STD. DWG. RP-H-3. FOR ADDITIONAL DETAILS AND OTHER INFORMATION FOR PERPENDICULAR CURB RAMPS NOT SHOWN ON THIS DRAWING SEE STD. DWG. RP-H-4 AND FOR PARALLEL CURB RAMPS SEE STD. DWG. RP-H-5. FOR CROSSWALK MARKING DETAILS SEE STD. DWG. T-M-4.
(B)	IF PERPENDICULAR CURB RAMPS AND TURNING SPACE CANNOT BE CONSTRUCTED DUE TO LIMITED RIGHT-OF-WAY, USE PARALLEL CURB RAMP INSTEAD.
(C)	CARE SHALL BE TAKEN ON ALL ROADWAY CURB RAMPS AT INTERSECTIONS WITH SIDEWALK AND CURB RAMPS TO ENSURE A UNIFORM GRADE AROUND THEM. THE ROADWAY CURB RAMP GRADE SHALL BE FREE OF SAGS AND SHORT GRADE CHANGES.
(D)	SIDEWALK WIDTH SHALL NOT INCLUDE 6" CONCRETE CURB. THE DESIRABLE SIDEWALK CROSS SLOPE IS 1.5 %, ABSOLUTE MAXIMUM IS 2.0%.
(E)	DRAINAGE STRUCTURES SHALL NOT BE PLACED IN THE CROSSWALK OR IN FRONT OF THE CURB RAMP.
(F)	TURNING SPACE \ CLEAR SPACE: CLEAR SPACE BEYOND THE BOTTOM GRADE BREAK, A CLEAR SPACE 4' (MIN.) BY 4' (MIN.) SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN STREET CROSSING AND WHOLLY OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE. TURNING SPACE MUST BE PROVIDED AT THE TOP OF PERPENDICULAR CURB RAMPS. THE TURNING SPACE MUST BE 4' (MIN.) BY 4' (MIN.), AND IS PERMITTED TO OVERLAP OTHER TURNING SPACES AND CLEAR SPACES. WHERE THE TURNING SPACE IS CONSTRAINED AT THE BACK OF THE SIDEWALK, THE TURNING SPACE MUST BE 4' (MIN.) BY 5' (MIN.), WITH THE 5' DIMENSION PROVIDED IN THE DIRECTION OF THE RAMP RUN. FOR PARALLEL CURB RAMPS, A TURNING SPACE 4' (MIN.) BY 4' (MIN.) SHALL BE PROVIDED AT THE BOTTOM OF THE CURB RAMP AND SHALL BE PERMITTED TO OVERLAP OTHER TURNING SPACES AND CLEAR SPACES. IF THE TURNING SPACE IS CONSTRAINED ON 2 OR MORE SIDES, THE TURNING SPACE SHALL 4' (MIN.) BY 5' (MIN.). THE 5' DIMENSION SHALL BE PROVIDED IN THE DIRECTION OF THE PEDESTRIAN STREET CROSSING.
(G)	GRADE BREAKS: GRADE BREAKS AT THE TOP AND BOTTOM OF CURB RAMP RUNS SHALL BE PERPENDICULAR TO THE DIRECTION OF THE CURB RAMP RUN. GRADE BREAKS SHALL NOT BE PERMITTED ON THE SURFACE OF CURB RAMP RUNS AND TURNING SPACES. SURFACE SLOPES THAT MEET AT GRADE BREAKS SHALL BE FLUSH. WHERE THE END OF BOTTOM GRADE BREAK ARE BEHIND THE BACK OF CURB AND THE DISTANCE FROM EITHER END OF THE BOTTOM GRADE BREAK TO THE BACK OF CURB IS 5' OR LESS, DETECTABLE WARNING SURFACES SHALL BE PLACED ON THE CURB RAMP RUN WITHIN ONE DOME SPACING OF THE BOTTOM GRADE BREAK.
(H)	PEDESTRIAN SIGNAL PUSHBUTTON: WHERE PEDESTRIAN SIGNALS ARE PROVIDED AT PEDESTRIAN STREET CROSSINGS, THEY SHALL INCLUDE ACCESSIBLE PEDESTRIAN SIGNALS AND POLE/PEDESTRIAN PUSHBUTTONS COMPLYING WITH SECTIONS 4E.08 THROUGH 4E.13 OF THE MUTCD. FOR DETAILS OF THE PLACING OF PEDESTRIAN SIGNAL PUSHBUTTONS SEE TDOT TRAFFIC DESIGN MANUAL. WHEN TWO PEDESTRIAN PUSHBUTTONS ARE ON ONE CORNER THEY WILL BE SEPARATED BY 10 FEET.
(I)	PAYMENT: COST OF CURB AND GUTTER TO BE INCLUDED IN THE PRICE OF ITEM NO. 702-01, CONCRETE CURB, PER C. Y. OR ITEM NO. 702-03, CONCRETE COMBINED CURB & GUTTER, PER C. Y. NEW CURB RAMPS: ALL COSTS OF INSTALLING CURB RAMP(S), INCLUDING DETECTABLE WARNING SURFACE(S) IN NEWLY CONSTRUCTED SIDEWALK AREAS, SHALL BE PAID BY ITEM NO. 701-02.03, CONCRETE CURB RAMP, PER SQUARE FOOT. PAYMENT SHALL INCLUDE ALL MATERIALS, EQUIPMENT, AND LABOR NECESSARY FOR CONSTRUCTION OF THE CURB RAMP(S), INCLUDING INSTALLATION OF DETECTABLE WARNING SURFACE(S). CURB RAMPS (RETROFIT): ALL COSTS OF INSTALLING CURB RAMP(S), INCLUDING DETECTABLE WARNING SURFACE(S) IN EXISTING SIDEWALK AREAS, REMOVAL OF THE EXISTING SIDEWALK, AND ADJUSTMENT OF GUTTER PAN SLOPE, SHALL BE PAID BY ITEM NO. 701-02.01, CONCRETE CURB RAMP (RETROFIT), PER SQUARE FOOT. PAYMENT SHALL INCLUDE ALL MATERIALS, EQUIPMENT, AND LABOR INSTALLATION OF CURB RAMP(S), INCLUDING INSTALLATION OF DETECTABLE WARNING SURFACE(S).

REV. 4-13-11: ADJUSTED CROSSWALK MARKINGS, ADDED TYPE 2 SIDEWALK DIMENSION, MISC. EDITS TO DRAWING.

REV. 5-8-13: REVISED TITLE FOR TERMINOLOGY.

REV. 6-4-13: REVISED NOTE © AND ⑩, CHANGED TITLE.

REV. 1-15-14: REMOVED ALTERNATE, AND REPLACED NOTE ⑥.

REV. 2-5-16: RENAMED TITLE. REMOVED ALTERNATE. MINOR DRAFTING EDITS.

REV. 10-10-16: ADDED GENERAL NOTES, MISC. REVISIONS TO DRAWINGS.

REV. 07-16-18: REMOVED RAMP FLARES IN 3 INSTANCES AND REPLACED WITH VERTICAL CURB. ADDED PARALLEL CURB RAMP OUTSIDE RADIUS DETAIL FROM STD. DWG. RP-H-9. REVISED GENERAL NOTES. REMAMED AND REDREW SHEET.

MINOR REVISION -- FHWA APPROVAL NOT REQUIRED

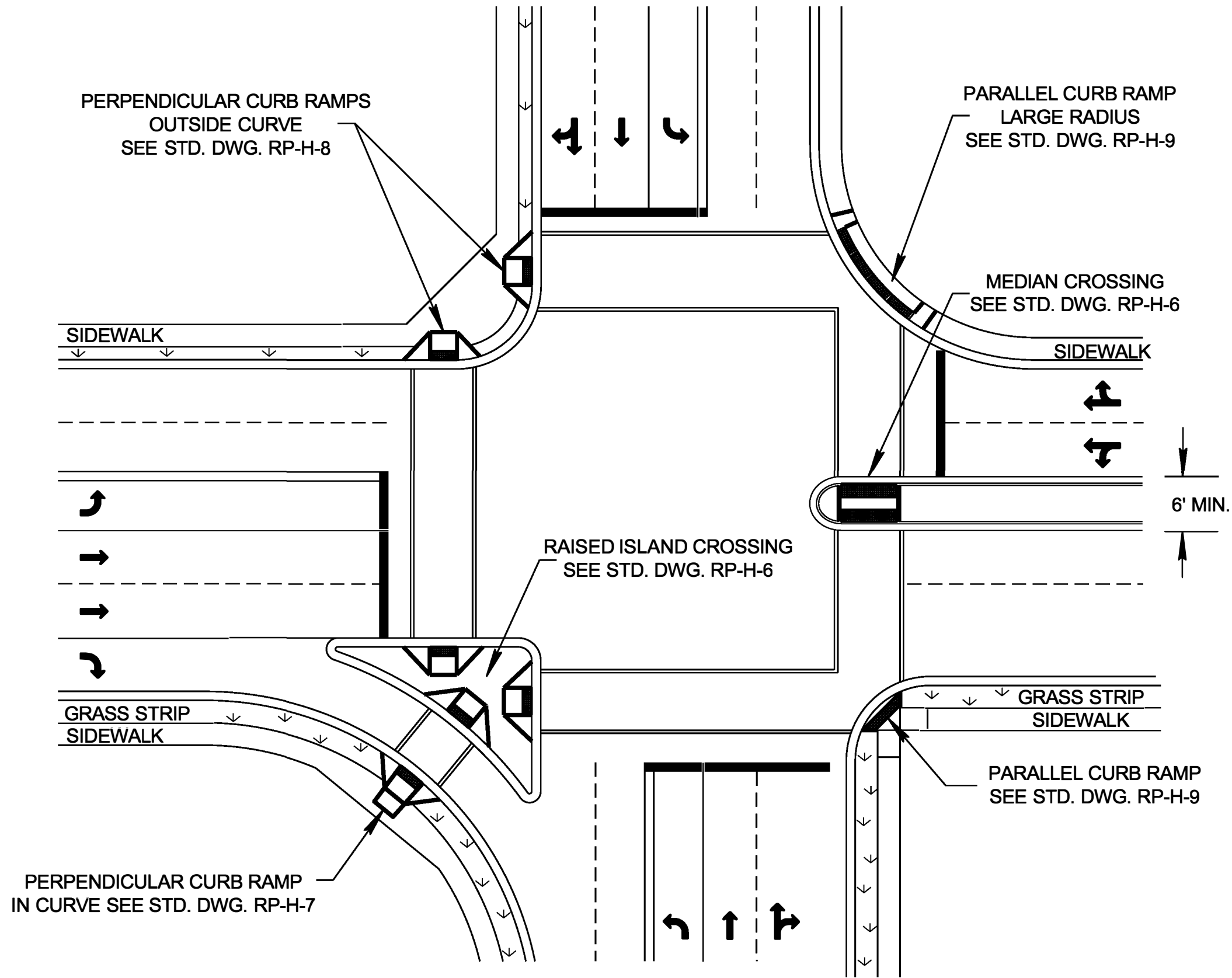
STATE OF TENNESSEE
DEPARTMENT OF
TRANSPORTATION

DUAL CROSSING
CURB RAMP
PLACED
OUTSIDE CURVE

01-15-07 RP-H-8

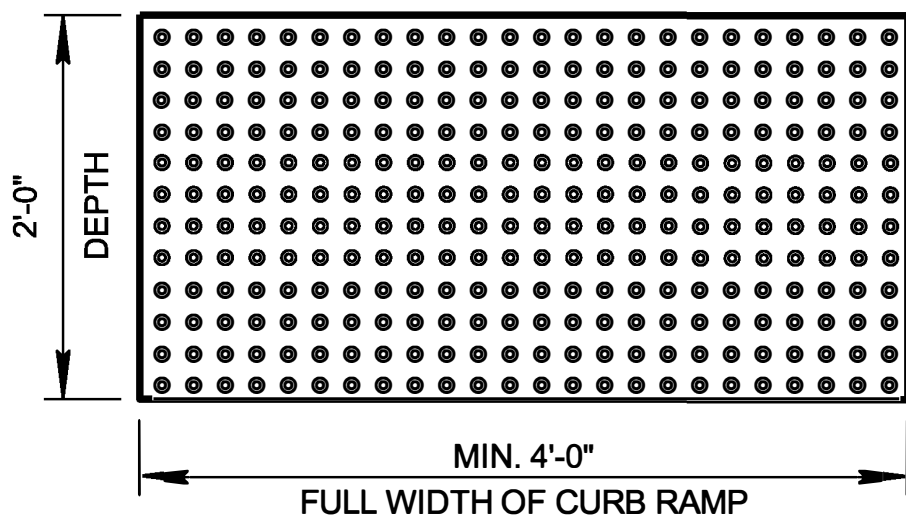
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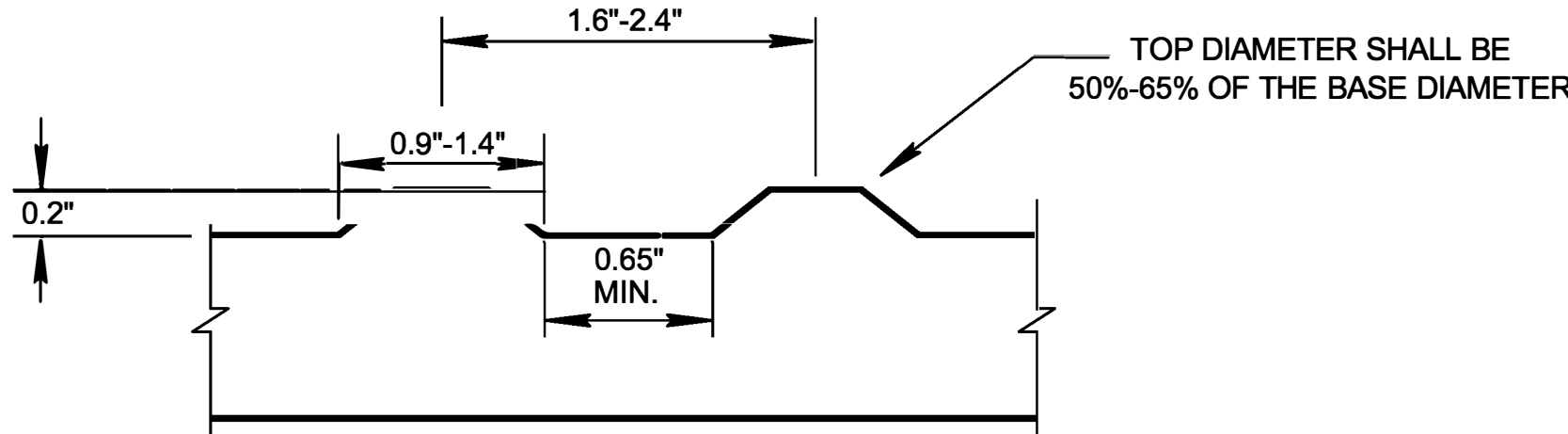


PLAN VIEW

(EXAMPLE LAYOUT SHOWING EACH RAMP TYPE; SEE REFERENCED STANDARD DRAWINGS FOR SPECIFIC ALIGNMENT INFORMATION)



DETECTABLE WARNING SURFACE DETAIL



DETECTABLE WARNING SURFACE
ELEVATION VIEW (TYP.)

GENERAL NOTES

- (A) DETAILS SHOWN ON THIS STANDARD DRAWING APPLY TO THE CONSTRUCTION OR RECONSTRUCTION OF STREETS, CURBS, OR SIDEWALKS.
- (B) **THE DETECTABLE WARNING SURFACES SHALL BE YELLOW.** THE COLOR YELLOW IS USED BECAUSE YELLOW IS THE LAST COLOR A VISUALLY IMPAIRED PERSON CAN DETECT PRIOR TO TOTAL LOSS OF VISION. DETECTABLE WARNING SURFACES SHALL BE CONSTRUCTED USING PRODUCTS APPROVED ON THE TDOT QUALIFIED PRODUCT LIST 37.
- (C) THE DEPTH OF DETECTABLE WARNING SURFACES SHALL BE 2 FEET, IN THE DIRECTION OF PEDESTRIAN TRAVEL. AT CURB RAMPS AND BLENDED TRANSITIONS, DETECTABLE WARNING SURFACES SHALL EXTEND THE FULL WIDTH OF THE RAMP RUN (EXCLUDING ANY FLARED SIDES), BLENDED TRANSITION, OR TURNING SPACE.
- (D) THE DETECTABLE WARNING SURFACE SHALL NOT BE EXTENDED BEYOND CROSSWALK BOUNDARIES AT LOCATIONS LACKING PROPER CURB HEIGHT.
- (E) CURB RAMPS ARE TO BE LOCATED AS SHOWN ON THE PLANS.
- (F) CURB RAMPS SHALL BE PROVIDED AT ALL CORNERS OF STREET INTERSECTIONS WHERE THERE IS EXISTING OR PROPOSED SIDEWALK AND CURB. CURB RAMPS SHALL ALSO BE PROVIDED AT MIDBLOCK CROSSWALK LOCATIONS AND ACROSS FROM CORNER RAMPS AT T-INTERSECTIONS.
- (G) CARE SHALL BE TAKEN TO ENSURE A UNIFORM GRADE ON THE RAMP. THE GRADE SHALL BE FREE OF SAGS AND SHORT GRADE CHANGES.
- (H) DRAINAGE STRUCTURES SHALL NOT BE PLACED IN LINE WITH RAMPS. INSTALL CATCH BASINS ON UPSTREAM SIDE OF RAMP FOR ROADS WITH GRADES LESS THAN 2%.
- (I) CROSSWALK MARKINGS, IF USED, SHALL BE LOCATED AS SHOWN ON THE APPLICABLE CURB RAMP STANDARD DRAWING. FOR CROSSWALK MARKING DETAILS, SEE STD. DWG. T-M-4.
- (J) FOR PERPENDICULAR CURB RAMP DESIGN DETAILS, SEE STD. DWG. RP-H-4. FOR PARALLEL CURB RAMP DESIGN DETAILS, SEE STD. DWG. RP-H-5.
- (K) PAYMENT:
ALL COSTS OF INSTALLING CURB RAMP(S), INCLUDING DETECTABLE WARNING SURFACE(S) IN NEWLY CONSTRUCTED SIDEWALK AREAS, SHALL BE PAID BY ITEM NO. 701-02.03, CONCRETE CURB RAMP, PER SQUARE FOOT.
- (L) FOR SIGNALIZED INTERSECTIONS THAT REQUIRE PEDESTRIAN SIGNAL PUSHBUTTONS, SEE TDOT TRAFFIC DESIGN MANUAL FOR PLACEMENT DETAILS.

- REV. 7-17-07: REVISED SIZE AND SPACING OF TRUNCATED DOMES, ADDED NOTE (E). MODIFIED SPECIAL PAVER NOTES.
- REV. 4-13-11: ADDED LOWERED CURB FOOTNOTE (1) TO TRUNCATED DOME DETAIL. MISC. EDITS TO DRAWING.
- REV. 5-8-13: ADDED GUTTER SLOPE DETAIL AND REVISED NOTE (1). UPDATED TERMINOLOGY.
- REV. 6-4-13: CHANGED ITEM DESCRIPTION. CLARIFIED NOTE (1), ADDED NOTE (2). ADDED SECTION A-A.
- REV. 11-25-13: REVISED NOTES (C), (F), (M), (N) AND ADDED NOTE (R).
- REV. 1-30-15: VARIOUS DRAFTING EDITS. REVISED GENERAL NOTES.
- REV. 10-10-16: REVISED NOTES AND TRUNCATED DOME SURFACE NOTES.
- REV. 07-16-18: REVISED GENERAL NOTES. MOVED TRUNCATED DOME SURFACE NOTES TO GENERAL NOTES. RENAMED AND REDREW SHEET.

MINOR REVISION -- FHWA
APPROVAL NOT REQUIRED

STATE OF TENNESSEE
DEPARTMENT OF
TRANSPORTATION

DETECTABLE
WARNING SURFACE
PLACEMENT ON
CURB RAMPS

01-15-07

RP-H-3

NOT TO SCALE

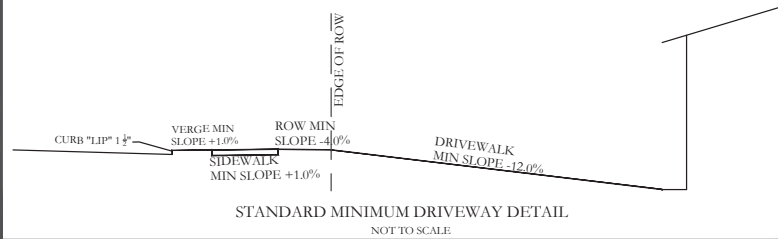
*MAXIMUM DECREASE IN ELEVATION FROM DRIVEWAY
LIP TO DRIVEWAY PAD IS 35.50" FOR TYPICAL 25' SET
BACK AND 5' SIDEWALK

*REQUIRED TO MAINTAIN A MINIMUM OF +1.0% SLOPE
AND 1.5' DRIVEWAY LIP FROM THE EDGE OF ROADWAY
TO THE BACK OF THE SIDEWALK

*SIDEWALKS ARE 5" OF CONCRETE AND 5" STONE

*DRIVEWAY WITHIN THE VERGE IS 6" OF CONCRETE
AND 6" OF STONE

*DRIVEWAY BETWEEN SIDEWALK AND EDGE OF ROW IS
5" OF CONCRETE AND 5" OF STONE

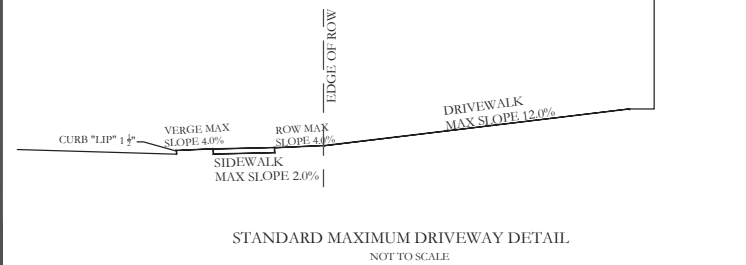


*MAXIMUM INCREASE IN ELEVATION FROM DRIVEWAY
LIP TO DRIVEWAY PAD IS 42" FOR TYPICAL 25' SET BACK
AND 5' SIDEWALK

*SIDEWALKS ARE 5" OF CONCRETE AND 5" STONE

*DRIVEWAY WITHIN VERGE IS 6" OF CONCRETE AND 6"
OF STONE

*DRIVEWAY FROM SIDEWALK TO EDGE OF ROW IS 5" OF
CONCRETE AND 5" OF STONE



0	INITIAL ISSUE	07/18/22	DATE OF ORIGINAL ISSUE	STANDARD NUMBER : <i>HCSD-200.03</i>	
NO.	REVISION	DATE	<i>JULY 18, 2022</i>		

HAMILTON COUNTY
PRIVATE DRIVEWAY ENTRANCE